



DEVELOPMENT and PLANNING SERVICES COMMITTEE

March 5, 2018

City of Salmon Arm

Council Chamber

City Hall, 500 - 2 Avenue NE

8:00 a.m.

Page #	Section	Item#
	1.	<u>CALL TO ORDER</u>
	2.	<u>REVIEW OF THE AGENDA</u>
	3.	<u>DECLARATION OF INTEREST</u>
	4.	<u>PRESENTATION</u> n/a
	5.	<u>REPORTS</u>
1 - 26	5.1	ZON-1118, Reimer, R. & R., 791 – 5 Street SE – R-1 to R-4
27 - 44	5.2	ZON-1115 & VP-468, Unruh, D. & R. / Hindbo Construction Group Ltd., 4431 – 17 Street NE – R-9 to R-8, Height Variances
45 - 52	5.3	VP-469, McKinnon, B. / Walters, R., 2820 – 30 Street NE – Setback Variance
	6.	<u>FOR INFORMATION</u> n/a
	7.	<u>IN CAMERA</u> n/a
	8.	<u>LATE ITEM</u> n/a
	9.	<u>ADJOURNMENT</u>

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City of Salmon Arm

Development Services Department Memorandum

To: Her Worship Mayor Cooper and Members of Council

Date: February 26, 2018

Subject: Zoning Bylaw Amendment Application No. 1117

Legal: Lot 5, Section 14, Township 20, Range 10, W6M, KDYD, Plan 5725
 Civic: 791 – 5 Street SE
 Owner/Applicant: Reimer, R. & R.

MOTION FOR CONSIDERATION

THAT: A bylaw be prepared for Council's consideration, adoption of which would amend Zoning Bylaw No. 2303 by rezoning Lot 5, Section 14, Township 20, Range 10, W6M, KDYD, Plan 5725 from R-1 (Single Family Residential Zone) to R-4 (Medium Density Residential Zone).

AND THAT: Final reading of the Bylaw be withheld subject to approval of the Bylaw by the Ministry of Transportation and Infrastructure.

STAFF RECOMMENDATION

THAT: The motion for consideration be adopted.

BACKGROUND

The 0.28 hectare subject parcel is located at 791 – 5 Street SE, just north of 10 Ave SE/Auto Road (Appendix 3 and 4), and has frontage on both 5 Street and 9 Avenue SE. The proposal is to rezone the parcel from R-1 (Single Family Residential Zone) to R-4 (Medium Density Residential Zone) to facilitate an 8-unit multi-family development.

This proposal was previously before Council in a staff report dated May 7, 2017, with a 9-unit development proposed. That proposal has been revised, with the current intent to develop 8-units. The applicant has submitted a letter and new site plans detailing their revised approach (Appendix 1), as well as a traffic analysis (Appendix 2).

The subject parcel is designated High Density Residential in the City's Official Community Plan (OCP) and zoned R-1 (Single Family Residential) in the Zoning Bylaw (Appendix 5 and 6). This area is largely comprised of R-1 zoned parcels containing single family dwellings, with R-4 and R-5 multi-family development further west and north. The parcel currently contains a single-family home.

The Zoning Map attached shows the mix of zones in the immediate area, predominantly Residential (R-1, R-4, R-5, and R-8), with Institutional zones to the west and undeveloped A-2 land further to the south. Land uses adjacent to the subject parcel include the following:

- North: Single-Family Residential (R-1) parcel,
- South: Road (9 Ave SE), with Single-Family Residential (R-1) parcels beyond,
- East: Single-Family Residential (R-1) and Residential Suite (R-8) parcels, with R-1 beyond, and
- West: Rural Holding (A-2) parcel, with R-4 and R-5 parcels beyond.

A conceptual site plan illustrating a total of 8 units in the form of 4 duplex buildings has been provided (Appendix 1), which would be subject to a future Development Permit application. Site photos are attached as Appendix 7.

OCP POLICY

The subject parcel is designated High Density Residential in the OCP, and is within Residential Development Area A, the highest priority area for development. While the proposed R-4 Medium Density zone is of slightly higher density (40 dwelling units per hectare) than current R-1 zoning, it is considerably less dense than envisioned by the High Density land use designation (100 dwelling units per hectare). In terms of managing growth, the long-term consequence of developing High Density designated lands at a Medium Density would be future pressure to expand the Urban Containment Boundary.

However, the proposed density aligns with OCP Policy 4.4.3, which encourages all growth to be sensitively integrated with neighbouring land uses. Furthermore, the proposed zoning aligns with the Urban Residential Objectives of Section 8.2 and Urban Residential Policies listed in Section 8.3, including providing a variety of housing types, providing housing options, and supporting compact communities. In terms of siting, the proposal appears to match with OCP Siting Policies under Section 8.3.19, including good access to transportation routes, recreation, community services, and utility servicing.

As per OCP residential policy, the multi-family development proposed would be subject to a future Development Permit application

OCP Map 11.2 designates a proposed greenway crossing over the south-west corner of the subject property (Appendix 8). As per OCP policy 11.3.18, the requirement of land dedication for a trail may be made at the discretion of Council.

COMMENTS

Ministry of Transportation and Infrastructure

Pursuant to the *Transportation Act*, approval of the zoning amendment bylaw by the Ministry is required, as the parcel is within 800 m of a Controlled Access Highway (Trans Canada Highway). The Ministry has granted Preliminary Approval for this rezoning.

Engineering Department

While not conditions of rezoning, full municipal services are required, including service upgrades, improvements to 5 Street SE, and a reciprocal access agreement to protect the neighbour's access from 851 5 Street SE.

The Engineering Department concurs with the conclusions of the Franklin Engineering Traffic and Access Assessment that the proposed access meets all safety requirements and that additional traffic flows are negligible. The Engineering Department also agrees with the report's recommendation to include a suitable onsite turn around to prevent vehicles exiting the driveway in reverse.

The attached comments have been provided to the applicant (Appendix 9).

Building Department

Setback variance may be required from access route at unit 7.

Internal access route length appears to trigger Fire Department concerns regarding turnaround.

No additional concerns with rezoning. Demolition permit required for existing building. Further review of limiting distance between units required at time of development.

Fire Department

Dead end roadways in excess of 90 metres require a turnaround as per BCBC 9.10.20.3(1) reference A 3.2.5.6.(1).

The attached comments have been provided to the applicant (Appendix 10).

Planning Department

Keeping in mind the High Density Residential OCP designation, the subject parcels are located in an area well-suited for higher density residential development with either R-4 or R-5 zoning, within walking distance to the City Centre. The maximum residential density permitted under R-4 (Medium Density) zoning is 40 dwelling units per hectare of land. As the subject property is 0.28 hectares in area, the maximum permitted density under R-4 would be 11 dwelling units assuming: 1) some form of strata development; 2) the present gross areas of the subject parcel; and 3) no density bonus. The minimum parcel area for a single family dwelling is 300 square metres.

While from a growth management perspective, the best use and density would be some form of multi-family development, staff note that a single family dwelling is a permitted use in the proposed R-4 zone. It is the opinion of Staff that the proposal represents a reasonable balance between growth management principles while respecting existing land uses: the proposed density (currently 8 units are proposed, reduced from 9 units previously considered) appears sensitive to established neighbouring land uses, while representing an increase in density.

In terms of a future development scenario, the shape of the parcel presents some challenges. The site presents challenges relative to snow clearance, emergency access and turn-around traffic. Opportunity for on-street parking at this site is very limited, thus it is important that the proposed development meet or exceed parking requirements (the preliminary site plan provided indicates 16 parking spaces, above the required 12). Additionally, a screened refuse/recycling area would also be required. Planning staff concur with the Franklin report and the City's Engineering Department, recommending the requirement of an onsite turn-around.

Site plans, landscape details, and elevation drawings submitted at the development permit stage are required to illustrate how the applicant's proposed development would address such requirements relative to the subject parcel.

OCP attributes (Appendix 8) include a greenway trail along the west and east perimeters. While small relative to the larger greenway network, a potential greenway connection through the south-west corner of the parcel could be a significant component enabling a feasible connection in an area of steep slopes, upon such time that lands to the west are redeveloped. OCP Map 11.2 designates the proposed greenway over the subject property, and as such, OCP Policy 11.3.18 provides for the requirement of land dedication for a trail at the discretion of Council as a condition for rezoning. In effect, doing so would be a community Amenity Contribution, which ideally results from a negotiated agreement between a local government and owner/applicant.

OCP Policy 11.3.19 allows for the Approving Officer to require land dedication for a trail as a condition for subdivision (stratification). Given the need for field work associated with determining an appropriate trail alignment and in the absence of any development proposed for the parcel to the west, staff would suggest a right-of-way for a future trail be deferred at this rezoning stage, to be established as a condition at time of subdivision.

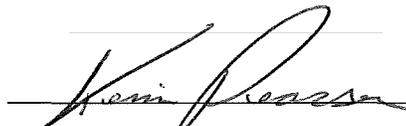
As previously noted, if rezoned to R-4, a form and character development permit application would be required prior to development to demonstrate how the proposed buildings, site and landscape designs will address the various requirements. A lot grading plan would be required at the development permit stage to confirm finished grades. Review of such an application would proceed through City staff, the Design Review Panel, and Council for consideration of approval.

CONCLUSION

The applicant has altered the initial proposal in a manner which improves practical functionality and should decrease associated impacts. The proposed R-4 zoning of the subject property is supported by OCP policy and is therefore supported by staff.



Prepared by: Chris Larson, MCP
Planning and Development Officer



Reviewed by: Kevin Pearson, MCIP, RPP
Director of Development Services

Roderick Reimer

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Enderby, BC V0E 1V3
250-833-8204
roderickreimer@gmail.com

Randal Reimer

875 Grandview Bench Rd
Salmon Arm, BC V1E 2X7
250-804-6205
randygreimer@hotmail.com

January 16, 2018

Mayor and Council
City of Salmon Arm
500 2 Avenue NE
Salmon Arm, BC V1E 4N2

RE: Rezoning application for property located at 791-5th ST SE

Dear Mayor and Council:

The purpose of this letter is to summarize our approach for the proposed development at the above-noted property, and to outline how feedback from Council, City staff, and concerned citizens has helped us to improve our plan since our initial application. This application was ultimately turned down on June 12, 2017, as you know.

Our intention is to create a positive, safe environment and a quality residential development which will be an asset to the neighborhood. Our current plan proposes density well below what is envisioned in the OCP, has more parking than is required by the Zoning Bylaw, provides viable access, and meets all Zoning setback requirements. We also plan to construct a fence around the development to ensure the privacy and security of the neighbors.

Improvements since our last application:

- Our previous plan proposed nine residential units. In our current plan we have reduced it to eight units. This allows for more parking, more green space, more space between buildings, and slightly less pressure on access and the street.
- We have included a traffic & access assessment completed by Franklin Engineering.
- We have a verbal agreement with the immediate neighbor regarding driveway access and hope to have a formal agreement in place shortly. (We have the support of at least one other nearby neighbor as well).

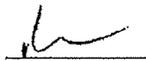
We feel these changes have resulted in an overall improved development plan and we look forward to working with you on this project.

Thank-you for your time and consideration.

Sincerely,



Roderick Reimer



Randal Reimer



PROJECT INFO:

Civic Address:
791 - 5th St. SE, Salmon Arm, BC

Legal Description:
Lot 5, Plan 8725, Sec. 14, Tp. 20, R. 10, W3M, K.O.V.D.

Site area:
2,851.4m (30,822.2sf)
0.705 acres

OCP Designation:
High Density Residential

Zoning:
Current - R-1 (Low Density Residential)
Proposed - R4 (Medium Density Residential)

Allowable Density:
16.2 units / acre = 11 units

Allowable Parcel Coverage:
55%

Maximum Building Height:
10.0m (32.8ft) for Principal Buildings
*13.0m (42.7ft) if amenities provided

Setbacks:

Front Yard	2.0m (6.6ft)
Rear Yard	5.0m (16.4ft)
Side Yard (Interior)	1.5m (5.0ft)
Side Yard (Exterior)	5.0m (16.4ft)

Min. Separation Between Residential Buildings on Same Lot:
- if not more than one storey = 1.5m (4.0ft)
- if more than one storey = 3.0m (9.8ft)

Required Parking:
1.5 Stalls / Unit

PROJECT SUMMARY:

Unit Totals:
2 x Duplex (2)
= 4 Units

Unit Types:

Units 1-4
Footprint = 70am (753.5sf) + Garage
Living Spaces on Main Floor
Bedrooms On Upper Floor
Walk-Out Basement
Approx. Gross Floor Area (2200sf)

Units 5-8
Footprint = 91am (980sf) + Garage
Living Spaces + Master Suite on Main Floor
Extra Bedrooms / Den / Utilities
Approx. Gross Floor Area (1900sf)

Units 7-8
Footprint = 78am (840sf), including Garage
Living Spaces on Second Floor
Bedrooms On Third Floor
Approx. Gross Floor Area (1950sf)

Parking:
Required = 12
Proposed = 16 (Units 1-6 with 1-Car Garage, Units 7-8 with 2-car Garage, plus 8 Surface)

Driveway Area:
Approx. 737sm (7,933sf)

PRELIMINARY

ISSUE	DATE



PROJECT:
Multi-Unit Residential Development
791 - 5th Street SE, Salmon Arm, BC

DRAWING TITLE:
SITE PLAN

DRAWING NO.:
A01

875 Grand View Bench Road
Salmon Arm, BC, V1E 2X7

January 25th, 2017

791 5th St SE Access Suitability Assessment

Dear Rodrick and Randal,

Franklin Engineering has undertaken a formal review of the access to the proposed 8 unit development at 791 5th St SE to determine its suitability to provide access to the proposed 8 unit development.

This has included a review of the road network, cycling and pedestrian requirements, minimum design requirements from the relevant bylaws and design guides, and an assessment of the site parameters.

Conclusions and recommendations have been provided at the end of the report.

Road Network Review

The City of Salmon Arm Official Community Plan (Map 12.1) identifies 5th Street SE as a designated Urban Collector Road. It is also noted that this map shows the proposed future east-west extension of Auto Road which is identified as an Urban Arterial Road. Until such time when this extension is constructed, during winter months the lower section of 10th St SE is closed and west bound traffic on Auto road is currently routed along 5th St SE.

The City of Salmon Arm Engineering Department has advised that no traffic volume data is available relevant to this access location.

The proposed development would be expected to generate an additional 64 trips/day (assuming traffic volumes of 8 trips/household /day typical of an urban environment).

Cycling and Pedestrian Review

5th St SE is not identified as a bike route in the OCP Cycle Network Plan (Map 12.2). The CoSA Subdivision and Development Servicing Bylaw provides a 1.5m wide bike lane in the typical cross section for an urban collector road (RD-3). A 1.2m Paved shoulder is provided on both side of the adjacent 5th St SE which is in general accordance with the bylaw requirements.

The CoSA Subdivision and Development Servicing Bylaw requires a 1.85m wide pedestrian sidewalk lane on both sides of the road in the typical cross section for an urban collector road (RD-3). A concrete pedestrian sidewalk is located on the adjacent (eastern) side of the 5th Ave NE. No sidewalk is currently on the western side, likely due to steep grades making it difficult and expensive to construct.

It is noted that the City's greenway/trail network crosses over the south-west corner of the parcel providing access to 9th Ave SE. OCP policy 11.3.18 may require this land to be dedicated for the extension of this trail network at the discretion of Council. This would provide an alternative access route for Pedestrians rather than crossing 5th St SE to the sidewalk on the adjacent side of the road.

Access Design Requirements

A review of the relevant COSA bylaws and Transport Association of Canada Design Guidelines (TAC) has been undertaken, to understand the minimum design requirements for the access.

A summary of the minimum design requirements from the CoSA Subdivision and Development Services Bylaw No. 4163 and the TAC is shown in table 1 below.

Table 1: Minimum Design Requirements

Design Parameter	Minimum design specification	Reference
Design Speed (km/hr) (HS Collector)	50	CoSA (Section 4.5)
Design Speed for vehicles approaching from south ¹ (km/hr)	30	TAC 2017 (Ch3, Eqn 3.2.3)
Maximum Centreline Grade (%)	12.0	CoSA (Section 4.9)
Maximum Driveway Grade (%) (Access length greater than 25m)	12.0	CoSA (Section 4.13.5)
Maximum Driveway Throat Grade (%) (First 3.5m of Driveway)	7.0	CoSA (Section 4.13.5)
Minimum Driveway Throat Width ² (Allowing 2 way traffic)(m)	4.5 (7.2)	TAC 2017 (Table 8.9.1)
Intersection Site Distance ³ from the north approach adjusted for 12% uphill grade (m)	80	TAC 2017 (Ch9, Eqn 9.9.1)

Intersection Site Distance ³ from the south approach adjusted for 12% downhill grade using 30km/hr design speed (50km/hr design speed shown in brackets) (m)	80 (130)	TAC 2017 (Ch9, Eqn 9.9.1)
Stopping Site Distance ⁴⁵ (m)	74	TAC 2017 (Ch2, Tb 2.5.3)

1. Design speed calculated using the travel path curve radius for a vehicle turning from 10th Ave SE onto 5th St SE. Design speed also field verified.
2. Recommended for driveways serving >4 units that a two way driveway is provided.
3. Noted that design values provided are based on the site triangle requirements for a formal intersection with a stop control on the minor road. TAC comments that for low use driveways, reduced sight distances are generally tolerable due to the low operating speeds and caution exercised by drivers.
4. Adjusted for downgrade.
5. It is noted that the 2017 Tac does not provide a specific stopping site distance for trucks, however comments that separate stopping site distance is not generally used in highway design as it is balanced through the truck driver having an eye height advantage and hence greater sight line. It is noted that Table 1.2.5.4 of TAC 2007 includes a stopping site distance for trucks with conventional braking of 85-110m.

Assessment of Site Access Parameters

Onsite parameters have been calculated with measurements taken from 3d software using a surface constructed from topographic survey and LIDAR survey information. These measurements have then been field verified. A summary of the parameters observed onsite is provided in table 2. A drawing showing grades and site lines and photographs taken from site are attached with this report.

Table 2: Proposed Access Site Parameters

Design Parameter	Value	Min/Max Design Requirement
5 th ST SE Centreline Grade ¹ (%)	12.1	<12
10 th Ave SE Centreline Grade (Approach) (%)	13.6	<12
9 th Ave SE Centerline Grade (%)	19.2	<12
Existing Driveway Grade (%)	2.4	<12
Existing Driveway Throat Grade (%)	7.2	<7
Site Distance North ² (m)	110	>80
Site Distance South ² (m)	120	>80

1. Average Grade Measured across 70m frontage.
2. Site distance calculated assuming driveway throat grade will be raised to provide platform at 2%.

Discussion

Generally it is observed that the proposed development access meets the minimum specified design requirements.

Intersection sightlines are defined as the sight distance available from the decision point, where vehicles are required to stop on the intersecting road while drivers are looking left and right along the major roadway, before entering the intersection. The intersection sight distance is adequate when it allows the design vehicles to safely make all the maneuvers to enter the live road (e.g. left turns, right turns), and accelerate to 85% of the design speed without significantly affecting vehicles travelling on the main roadway.

A driver's eye setback of 3.0m from the road fog line has been used to define the decision point for intersection site distance calculation.

Looking north down 5th St SE, sightlines are clear and in excess of 120m. Minor trimming of the adjacent hedges (to the south) should be undertaken regularly to ensure the sightline remains clear and unobstructed.

Looking south towards the 10th Ave SE intersection, sightlines allow view of cars entering 5th St SE from 10th Ave SE. A revised design speed of 30km/hr has been calculated for northbound vehicles based on a reduced travel speed for vehicles that have just navigated the 5th St/10th Ave intersection corner (calculated using a 23m curve radius and field verified). Using this travel speed, the available sightline (110m) exceeds the minimum requirements (80m) including an adjustment for the road gradient.

It is noted that the gradient of the 5th St centerline varies, however the measured value represents the average gradient across a 70m frontage. The gradient (12.1%) slightly exceeds the maximum design gradient included in design guides; however is within reasonable tolerance levels, particularly given some inaccuracy in the survey model.

The driveway entrance should be upgraded to a width suitable to for two way access (>7.2m) and to provide a flat throat grade ($\pm 2\%$) to ensure a safe storage platform is provided for vehicles waiting to enter and exit the driveway.

The existing gradient of the driveway throat (first 5.0m) exceeds the recommended maximum grade. The provision of appropriate clear throat length or storage space is particularly important as it provides a flat storage platform for vehicles waiting to enter the live lane and for receiving vehicles leaving the live road. The driveway should be upgraded to provide a flat ($\pm 2\%$) platform at the entrance/exit.

The CoSA bylaw encourages developing shared driveways on adjacent properties to reduce the number of accesses on collector roads.

The additional vehicles generated by the proposed development would be expected to be negligible on a designated collector road.

In the long term, the future upgrade of Auto Road should result in a significant decrease in traffic volume on 5th St SE.

Recommendations/Conclusions

From a review of the minimum design requirements and an assessment of the site parameters the following recommendations are made:

1. The proposed access is in general accordance with the minimum design requirements. The gradient of 5th St SE (12.1%) is at the maximum recommended grade, however when adjusting for this grade, the sightlines and safe stopping distance available are in accordance with the requirements of a highways intersection and thus are considered suitable for a low volume residential access ;
2. The driveway entrance should be upgraded to a width suitable to for two way access (>7.2m) and to provide a flat throat grade ($\pm 2\%$) to ensure a safe storage platform is provided for vehicles waiting to enter and exit the driveway;
3. The internal driveway access should be of a suitable width, or provide a suitable turning area to allow passenger vehicles to exit the driveway in a forwards direction. Ie prevent vehicles from exiting driveway in reverse.
4. Consultation with adjacent property is being undertaken to develop a shared access, reducing the number of access points onto a collector road in accordance with the CoSA bylaw design requirements;
5. Minor trimming or removal of the adjacent trees (to the south) should be undertaken regularly to ensure the sightlines remains clear and unobstructed.
6. The additional traffic generation expected from the development is negligible in comparison to the traffic volumes expected on a collector road;
7. It is noted that an access off 9th Ave is not considered suitable due to its steep grade (max 19%) and reduced sightlines in both directions;

Please contact Franklin Engineering if you have any further questions regarding this access assessment.

Prepared by,

A handwritten signature in black ink, appearing to read 'S. Husband'.

Sean Husband, EIT

Reviewed by,

A handwritten signature in blue ink, appearing to read 'Jayme Franklin', is written over a circular professional seal. The seal contains the text 'PROFESSIONAL ENGINEER' around the perimeter, 'PROVINCE OF BRITISH COLUMBIA' in the center, and 'J. FRANKLIN # 34134' in the middle.

Jayme Franklin, P.Eng

Attached:

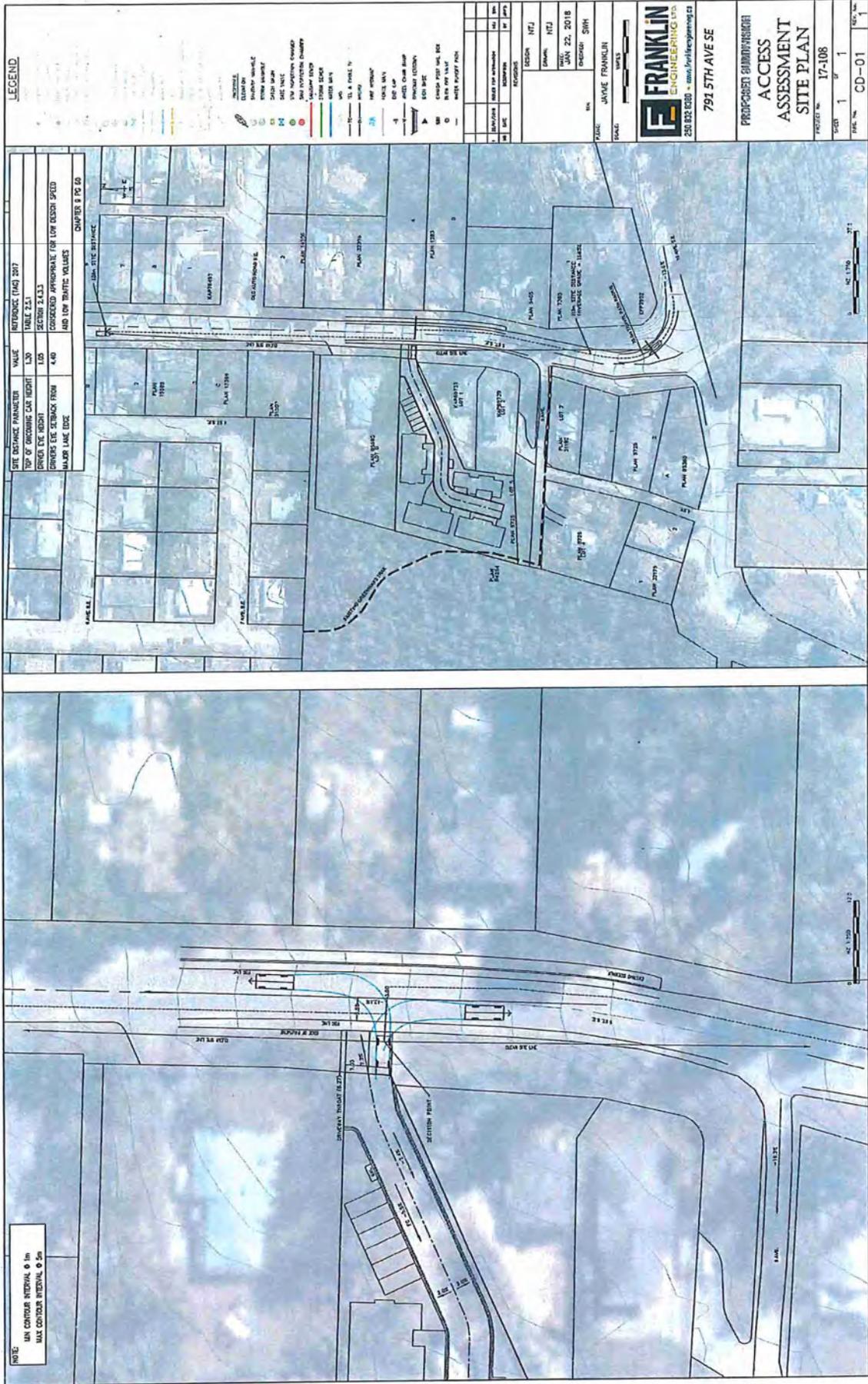
- Franklin Engineering Drawing 17-018 SK-01 Access Assessment Site Plan (Rev2)
- Sightline Photos

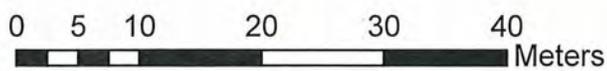
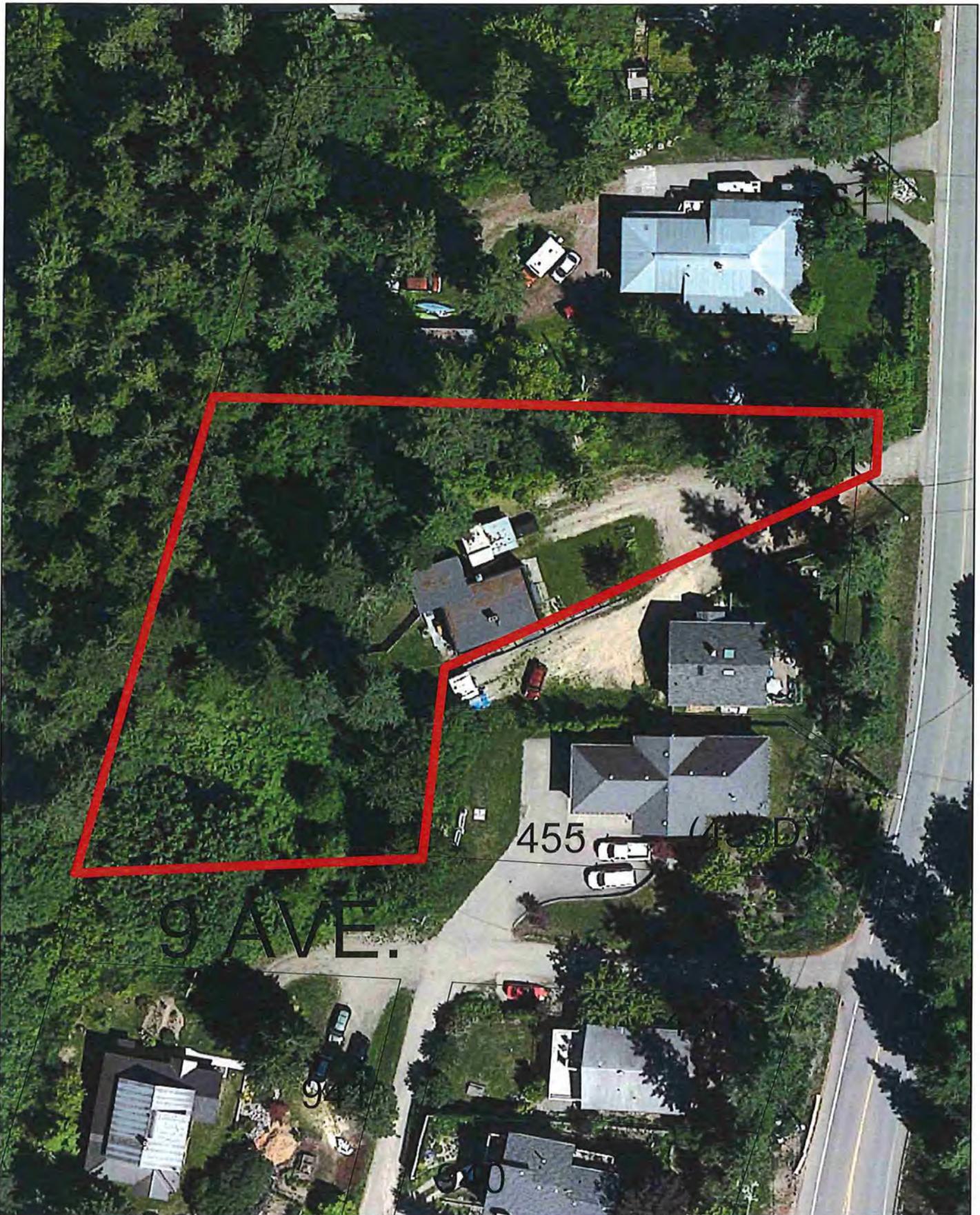


Figure 1: Sightline from Decision Point Looking North

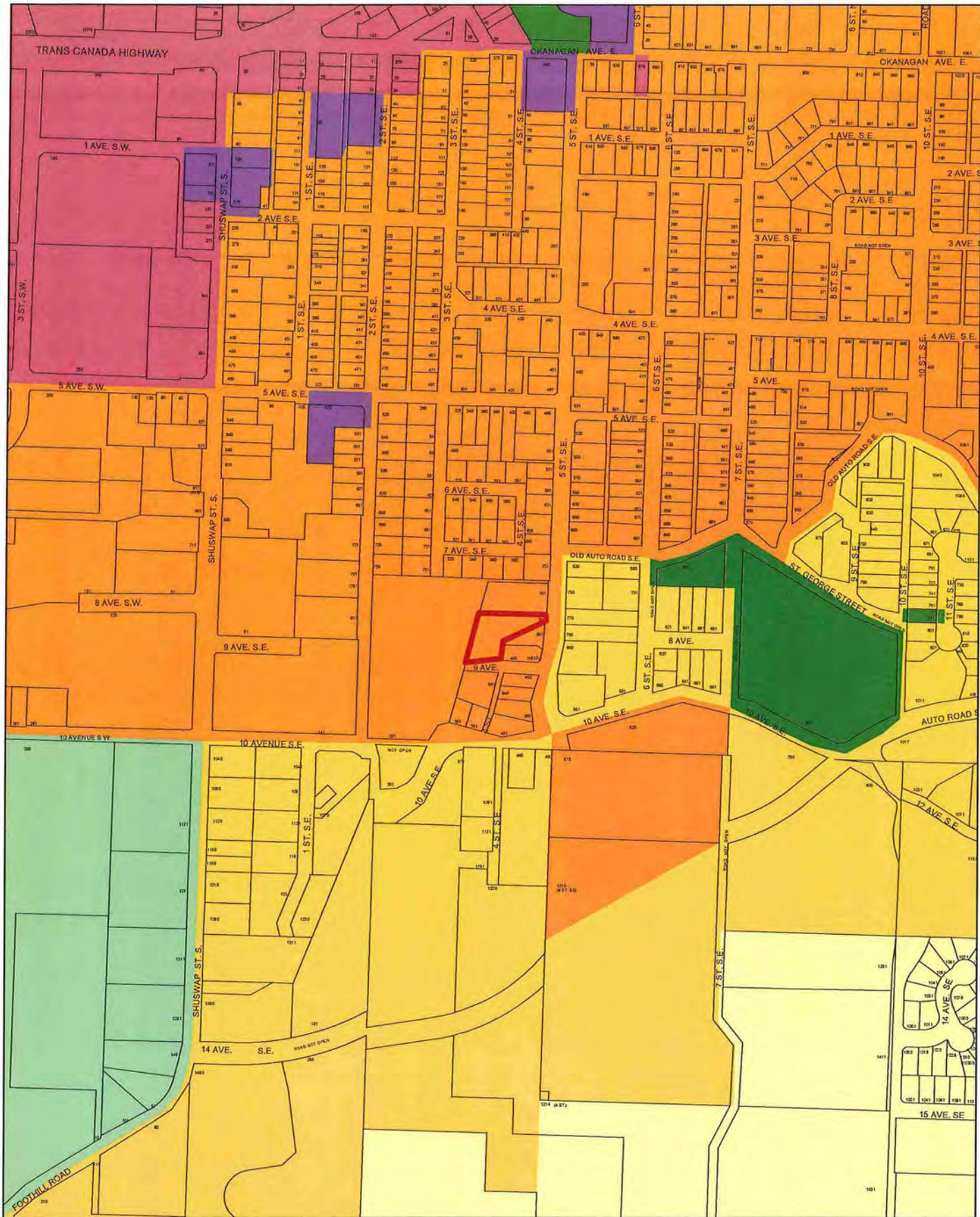


Figure 2: Sightline from Decision Point Looking South

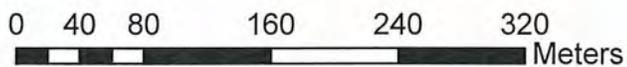
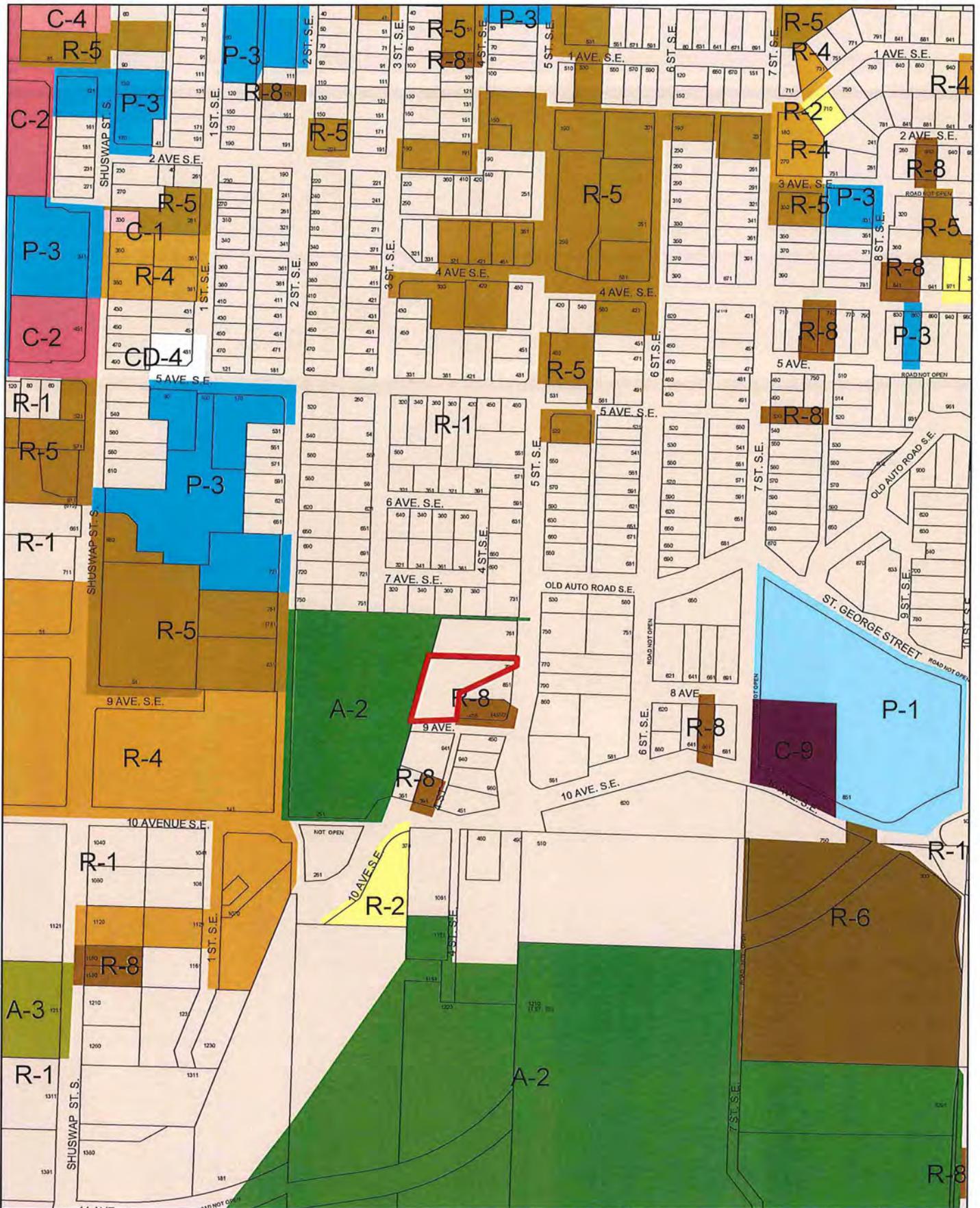




Subject Parcels



		Subject Parcel	Commercial - City Centre	High Density Residential
		Institutional	Park	Low Density Residential
		Acreage Reserve	Medium Density Residential	



Subject Parcels

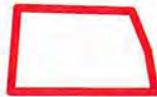
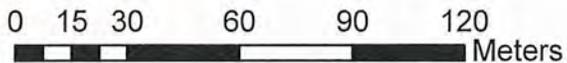
Appendix 7: Site Photos



View of subject parcel looking south-west from 5 Street SE, with neighbour's home at left.



View of rear portion of subject parcel looking north-west from 9 Avenue SE, with existing house visible..



Subject Parcels



**City of Salmon Arm
Memorandum from the Engineering
and Public Works Department**

TO: Kevin Pearson, Director of Development Services
 DATE: 21 February 2018
 PREPARED BY: Chris Moore, Engineering Assistant
 OWNER: **Reimer, Roderick**, 929 Musgrave Road, Enderby, BC V0E 1V3
Reimer, Randal, 875 Grandview Bench Road, Salmon Arm, BC V1E 2X7
 APPLICANT: Owners
 SUBJECT: **ZONING AMENDMENT APPLICATION FILE NO. ZON-1117**
 LEGAL: Lot 5, Section 14, Township 20, Range 10, W6M, KDYD, Plan 5725
 CIVIC: **791 – 5 Street SE**

Further to the request for Zoning Amendment dated 30 January 2018; the Engineering Department has thoroughly reviewed the site. The following comments and servicing requirements are not conditions for rezoning; however, these comments are provided as a courtesy in advance of any development proceeding to the next stages:

General:

1. Full municipal services are required as noted herein. Owner / Developer to comply fully with the requirements of the Subdivision and Development Services Bylaw No 4163. Notwithstanding the comments contained in this referral, it is the applicant's responsibility to ensure these standards are met.
2. Comments provided below reflect the best available information. Detailed engineering data, or other information not available at this time, may change the contents of these comments.
3. Properties to be serviced completely by underground electrical and telecommunications wiring.
4. Properties under the control and jurisdiction of the municipality shall be reinstated to City satisfaction.
5. Owner / Developer will be responsible for all costs incurred by the City of Salmon Arm during construction and inspections. This amount may be required prior to construction. Contact City Engineering Department for further clarification.
6. Erosion and Sediment Control measures will be required at time of construction. ESC plans to be approved by the City of Salmon Arm.
7. Any existing services (water, sewer, hydro, telus, gas, etc) traversing the proposed lot must be protected by easement or relocated outside of the proposed building envelope. Owner/Developer will be required to prove the location of these services. Owner / Developer is responsible for all associated costs.
8. At the time of development the applicant will be required to submit for City review and approval a detailed site servicing / lot grading plan for all on-site (private) work. This plan will show such items as parking lot design, underground utility locations, pipe sizes, pipe

ZONING AMENDMENT APPLICATION FILE NO. ZON-1117
21 February 2018
Page 2

elevations, pipe grades, catchbasin(s), control/containment of surface water, contours (as required), lot/corner elevations, impact on adjacent properties, etc.

9. For the off-site improvements at the time of development/building permit the applicant will be required to submit for City review and approval detailed engineered plans for all off-site construction work. These plans must be prepared by a qualified engineer. As a condition of building permit approval, the applicant will be required to deposit with the City funds equaling 125% of the estimated cost for all off-site construction work.

Roads / Access:

1. 5 Street SE, on the subject properties eastern boundary, is designated as an Urban Collector Road standard, requiring 20.0m road dedication (10.0m on either side of road centerline). Available records indicate that no additional road dedication is required (to be confirmed by a BCLS).
2. 5 Street SE is currently constructed to an Interim Collector Road standard. Upgrading to an Urban Collector Road Standard is required, in accordance with Specification Drawing No. RD-3. Upgrading may include, but is not limited to, road widening and construction, curb & gutter, sidewalk, boulevard construction, street lighting, fire hydrants, street drainage and hydro and telecommunications. Owner / Developer is responsible for all associated costs. In consideration of the narrow frontage onto 5 Street NE, this upgrade is premature at this time. A 100% cash in lieu payment towards future upgrading will be accepted. Owner / Developer is responsible for all associated costs.
3. 9 Avenue SE, on the subject properties southern boundary, is designated as a Local Road standard, requiring 20.0m road dedication (10.0m on either side of road centerline). Available records indicate that 9 Avenue is only 9.2m wide at the west end, however no additional dedication will be required since the existing grade is too steep to construct a road to City standards. For this reason, the full upgrade of 9 Avenue SE will not be required, however existing road drainage issues will need to be addressed.
4. 851 5 Street SE currently shares an access with the subject property. A reciprocal access easement will be required to protect the access of 851 5 Street SE.
5. Owner / Developer is responsible in ensuring all boulevards and driveways are graded at 2.0% towards the existing roadway.
6. The conclusions of the Franklin Engineering Traffic and Access Assessment are that the proposed access meets all safety requirements and that additional traffic flows on 5 Street SE will be negligible. The Engineering Department agrees with these conclusions. However, the report does also recommend that vehicles should be able to turn onsite to prevent exiting the driveway in reverse. A suitable turn-around should therefore be provided onsite to allow all vehicles to turn and exit in a forward direction. This would also permit the strata to opt in to the City's Garbage and Recycling Program.

ZONING AMENDMENT APPLICATION FILE NO. ZON-1117
21 February 2018
Page 3

Water:

1. The subject property fronts a 150mm diameter Zone 1 watermain on 5 Street SE. No upgrades will be required at this time.
2. The subject property fronts on 9 Avenue SE where no watermain is currently constructed. Since extending a watermain along 9 Avenue SE is premature at this time, a 100% cash contribution for the future construction of a watermain across the frontage on 9 Avenue SE will be required.
3. The property is to be serviced by a single water service connection, adequately sized to satisfy the proposed use. Strata lots shall have individual water meters that will be supplied by the City at the time of Building Permits. Owner / Developer is responsible for all associated costs.
4. Records indicate that the existing property is serviced by a 12.5mm service from the 150mm diameter watermain on 5 Street SE. All existing inadequate / unused services must be abandoned at the main. Owner / Developer is responsible for all associated costs.
5. The subject property is in an area with sufficient fire flows and pressures according to the 2011 Water Study (OD&K 2012).
6. Fire protection requirements to be confirmed with the Building Department and Fire Department.

Sanitary:

1. The subject property fronts a 200mm diameter sanitary sewer located on 5 Street SE and a 150mm diameter sanitary sewer located on 9 Avenue SE. Upgrading to a minimum 200mm diameter will be required across the frontage of the property on 9 Avenue SE.
2. The property is to be serviced by a single sanitary service connection adequately sized (minimum 100mm diameter) to satisfy the servicing requirements of the development. Owner / Developer is responsible for all associated costs.
3. Records indicate that the existing property is serviced by a 100mm service from the sanitary sewer on 5 Street SE. All existing inadequate/unused services must be abandoned at the main. Owner / Developer is responsible for all associated costs.

Drainage:

1. The subject property fronts a 600mm diameter storm sewer located on 5 Street SE and a 300mm diameter storm sewer located on 5 Street SE. There is no storm sewer located on 9 Avenue SE. Since 9 Avenue SE is not required to be upgraded, the provision of a storm sewer across the frontage of the property is not required. The owner / developer is however

ZONING AMENDMENT APPLICATION FILE NO. ZON-1117
21 February 2018
Page 4

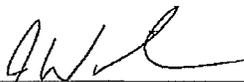
- required to provide a storm water system to address existing flows on 9 Avenue, to prevent these discharging onto private property. Owner / Developer is responsible for all associated costs.
2. An Integrated Stormwater Management Plan (ISMP) conforming to the requirements of the Subdivision and Development Servicing Bylaw No. 4163, Schedule B, Part 1, Section 7 shall be provided. Where onsite disposal of stormwater is recommended, a Stormwater Management System shall be provided in accordance with Section 7.2.
 3. Should discharge into the City Storm Sewer be part of the ISMP, the proposed lot(s) shall be serviced (each) by a single storm service connection adequately sized (minimum 150mm) to satisfy the servicing requirements of the development. Owner / Developer's engineer may be required to prove that there is sufficient downstream capacity within the existing City Storm System to receive the proposed discharge from the development. All existing inadequate / unused services must be abandoned at the main. Owner / Developer is responsible for all associated costs.

Geotechnical:

1. A geotechnical report in accordance with the Engineering Departments Geotechnical Study Terms of Reference for: Category A (Building Foundation Design), Category B (Pavement Structural Design), Category C (Landslide Assessment).



Chris Moore
Engineering Assistant



Jenn Wilson P.Eng., LEED® AP
City Engineer



Print

Submit Form

CITY OF SALMON ARM - DEVELOPMENT SERVICES DEPARTMENT
Box 40, 500 - 2nd Avenue NE, Salmon Arm, BC, V1E 4N2
Phone: 250-803-4021 FAX: 250-803-4041

TO: DIRECTOR OF DEVELOPMENT SERVICES (Kevin)
 PLANNING AND DEVELOPMENT OFFICER (Jon)
 PLANNING AND DEVELOPMENT OFFICER (Wes)
 PLANNING AND DEVELOPMENT OFFICER (Chris)
 MANAGER OF PERMITS & LICENSING (Maurice)
 FIRE DEPARTMENT (Brad)
 ENGINEERING & PUBLIC WORKS DEPARTMENT (Rob, Jenn & Jennifer)
 MINISTRY OF TRANSPORTATION & INFRASTRUCTURE, P.O. Box 100, Salmon Arm, BC, V1E 4S4
 BC HYDRO, via email utilities group
 FORTISBC, via email utilities group
 TELUS, via email utilities group
 SHAW CABLESYSTEMS, via email utilities group

REFERRAL

OWNER: Reimer, Roderick, [REDACTED]
 Reimer, Randal, [REDACTED]
 APPLICANT: Owner
 AGENT: Reimer, Roderick, [REDACTED]
 DATE: January 30, 2018
 SUBJECT: ZONING AMENDMENT APPLICATION FILE NO. ZON-1117
 LEGAL: Lot 5, Section 14, Township 20, Range 10, W6M, KDYD, Plan 5725
 CIVIC: 791 - 5 Street SE

Please provide comments on the attached Zoning Amendment Application at your earliest opportunity.

OCP Designation:	HR (High Density Residential Zone)
OCP Designation Requested:	n/a
Development Permit Area:	Environmentally Sensitive Riparian Area & Potential Hazardous Area
Current Zoning:	R-1 (Single Family Residential Zone)
Requested Zoning:	R-4 (Medium Density Residential Zone)
ALR:	No
Other:	The applicant wishes to build 4 duplexes
Previous Files:	ZON-1090
Associated File:	n/a

Thank you.

Kevin Pearson, MCIP, RPP
 Director of Development Services

COMMENTS for ZON-1117:

It appears the roadway of this development is in excess of 90 Meters.
 BCBC 9.10.20.3(1) references A 3.2.5.6.(1) and a requirement for a turnaround for
 dead end roadways in excess of 90 M.
 Being consistent with our past practice of such developments we would ask for a
 turnaround or T-turn at the end of the roadway.
 Thank you

SIGNATURE: Jim Nickles

DATE: Feb. 7/18



City of Salmon Arm

Development Services Department Memorandum

TO: Her Worship Mayor Cooper and Members of Council

DATE: February 22, 2018

SUBJECT: Zoning Bylaw Amendment Application No. 1115 (R-9 to R-8)
 Variance Permit Application No. VP-468 (Accessory Building and Retaining Wall Height)
 Legal: Lot 3, Section 25, Township 20, Range 10, W6M, KDYD, Plan 40035
 Civic Address: 4431 – 17 Street NE
 Owner: Dale and Renee Unruh
 Agent: Hindbo Construction Group Inc.

MOTION FOR CONSIDERATION

THAT: Development Variance Permit No. VP-468 be authorized for issuance for Lot 3, Section 25, Township 20, Range 10, W6M, KDYD, Plan 40035 which will vary Zoning Bylaw No. 2303 as follows:

1. Section 4.12.1 (a) Fences and Retaining Walls – increase the maximum height of a fence in conjunction with retaining walls from 2.0 m to 4.2 m as shown on Meyer Designs Ltd. dated January 30, 2018 (File No. 2017-043) and attached as APPENDIX 5.
2. Section 14.6 Maximum Height of Accessory Buildings – increase the maximum height of an accessory building from 6.0 m to 8.7 m to allow for a new accessory building, as shown on Meyer Designs Ltd. dated December 5, 2017 (File No. 2017-043-A) and attached as APPENDIX 5.

AND THAT: a bylaw be prepared for Council's consideration, adoption of which would amend Zoning Bylaw No. 2303 by rezoning proposed Lot B shown on the Subdivision Plan prepared by Brown Johnson Land Surveyors received January 19, 2018 (File No. 656 – 17) and attached as Schedule A from R-9 (Estate Residential Zone) to R-8 (Residential Suite Zone).

STAFF RECOMMENDATION

THAT: The motion for consideration be adopted.

PROPOSAL

The subject property is located at 4431 – 17 Street NE (APPENDICES 1 and 2). The proposal is to rezone the parcel from R-9 (Estate Residential) to R-8 (Residential Suite) to allow for a detached suite to be constructed. The applicant is also requesting two variances, first to increase the maximum height of four retaining walls, ranging from 2.0 m to 4.2 m, and second to increase the maximum height of an accessory building from 6.0 m to 8.7 m to accommodate the proposed detached suite.

In addition, the applicant has applied for a boundary adjustment (File: 18.04). This is to accommodate construction of an upgraded driveway, retaining walls and garage addition.

The proposed subdivision plan, site layout and building elevations are attached as APPENDIX 5. A letter of rationale is attached as APPENDIX 6 and site photos are attached as APPENDIX 7.

COMMENTS

Fire Department

No concerns.

Building Department

No concerns.

Engineering Department

No concerns.

Planning Department

The applicant is requesting to rezone the subject property in addition to requesting to variances to the Zoning Bylaw.

Residential Suite Zone (R-8)

The subject parcel is designated Low Density in the City's Official Community Plan (APPENDIX 3) and zoned R-9 (Estate Residential) in the Zoning Bylaw (APPENDIX 4). The area is largely comprised of R-7, R-9 and R-1 zoned parcels containing single family dwellings, however there are eight R-8 zoned properties within the Raven Hill subdivision area.

The applicant is requesting to rezone the subject property from R-9 to R-8 to allow for the construction of a detached suite. The proposed lot meets all the conditions of minimum parcel area and minimum parcel width as specified to permit a detached suite. The site has a number of options to accommodate the required additional parking stall.

Fences and Retaining Walls

The Zoning Bylaw allows the maximum height of a retaining wall to be 2.0 m in a residential zone. The design of the site shows four large retaining walls which vary in height and at one point all will be above the maximum. In addition, the retaining wall along the pool deck includes an estimated 1.2 m railing which brings its combined maximum height to 4.2 m. The purpose of the retaining walls is to support the construction of multiple structures on the sloping site, including a detached suite, pool, poolside cabana, and garage addition to the existing single family dwelling.

There are three properties directly adjacent to the west which may incur a visual impact from the proposed development. Currently there is limited buffering along the shared property line, and the initial site preparation is visible from the Lakeshore Road down slope.

Height of Accessory Building

The applicant is requesting an increase to the maximum height of an accessory building from 6.0 m to 8.7 m, for a variance of 2.7 m. In general terms, building height is measured from the mid-point of the lowest existing grade adjacent to the buildings foundation. For the proposed building, that existing grade point is measured vertically from where west facing foundation wall would be sited.

The height of the proposed building is 8.7 m; with most of the variance accounting for the raise in the grade and slope of the building site. The front elevation is roughly 4.2 m and gives the detached suite a

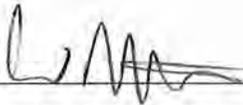
single story look from the 17 Street NE frontage. The accessory building is intended to be used as a detached suite if the associated zoning amendment is approved by Council. The structure would still be 1.2 m over the maximum permitted height of 7.5 m of a detached suite in the R-8 zone.

Staff is recommending approval in consideration of the following:

1. The proposal is a comprehensive residential development involving a number of structures on a significantly sloped lot. In general, an increased height to the accessory building (future detached suite) and identified retaining walls is reasonable due to site topography and scope of development.
2. The structures will be significantly outside the 6.0 m required rear yard setback however there is limited existing vegetation and mature trees acting as a visual buffer to the adjacent neighbours down slope to the west. The structures may result in some visual impacts to the neighbouring lots. There are no anticipated negative impacts on views from the surrounding properties upslope of the subject property.

CONCLUSION

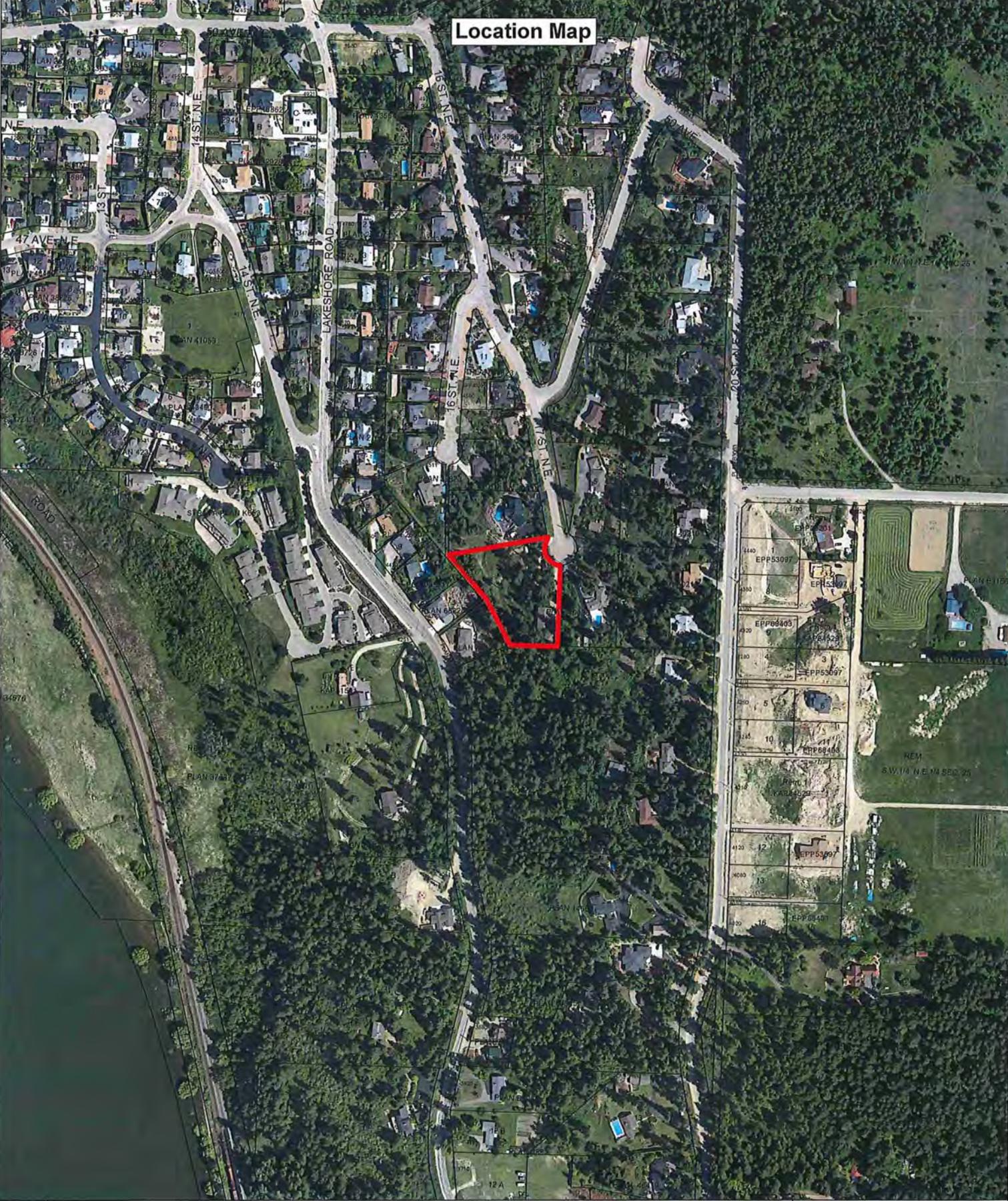
The requested zoning amendment to R-8 (Suite Residential) and the requested variance for height of an accessory building and fence/retaining walls is recommended for approval by staff for the above noted reasons.



Prepared by: Wesley Miles, MCIP, RPP
Planning and Development Officer



Reviewed by: Kevin Pearson, MCIP, RPP
Director of Development Services



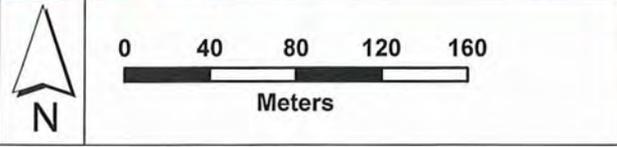
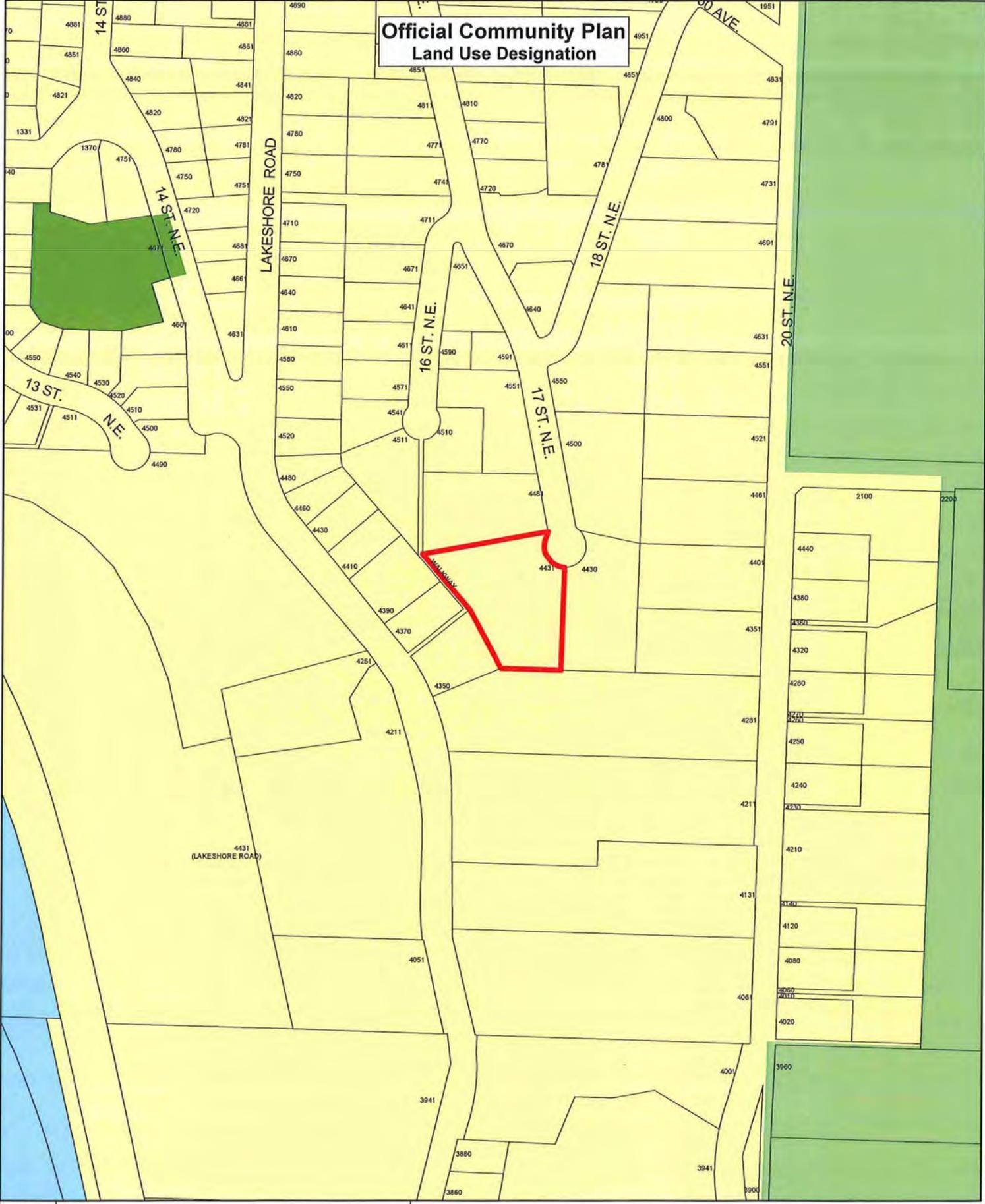
Location Map

0 50 100 150 200
Meters

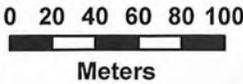
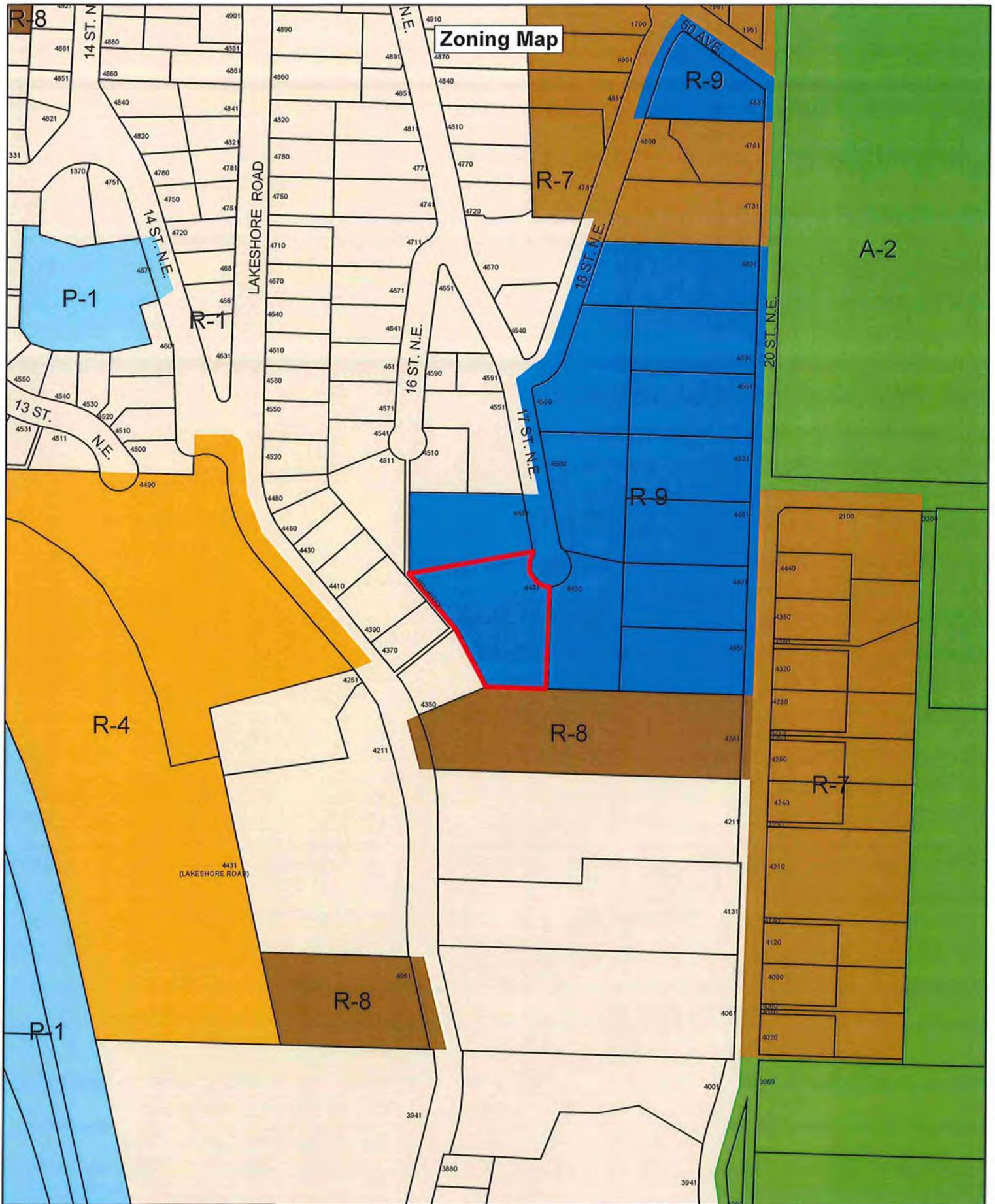
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 Subject Parcel

**Official Community Plan
Land Use Designation**



Zoning Map



Subject Parcel

Estate Residential

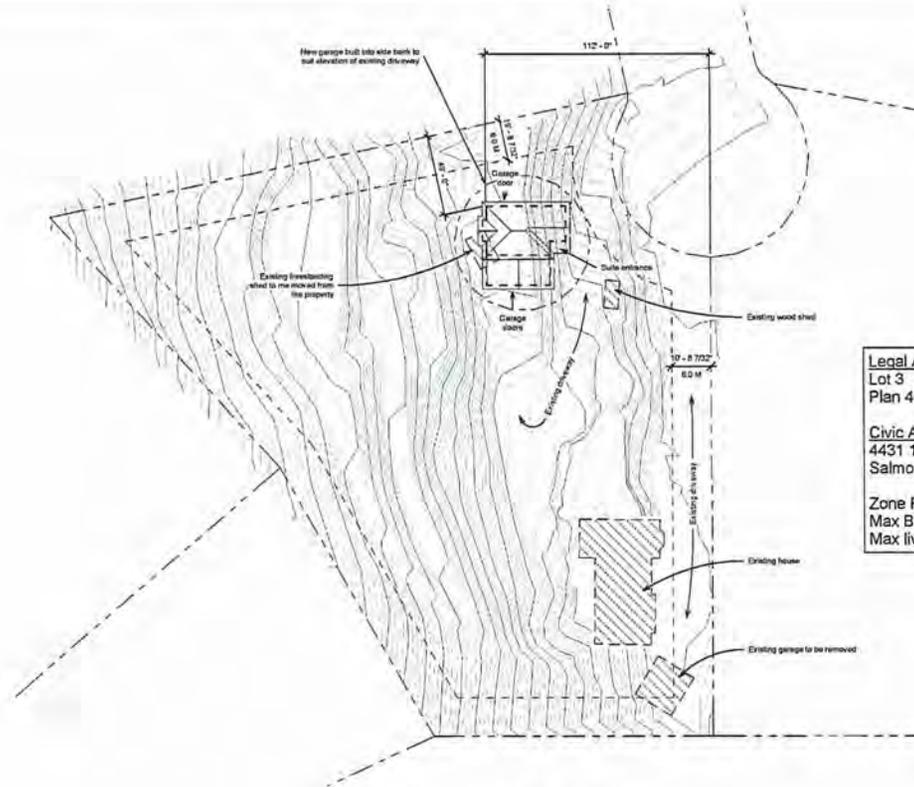
Residential Suite

Single Family Residential

Park and Recreation

Medium Family Residential

Large Lot Residential



① Site Plan
1" = 30'-0"

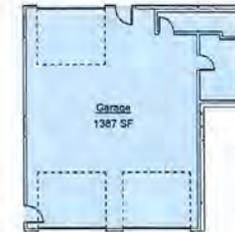
Legal Address
Lot 3
Plan 40035

Civic Address
4431 17th Street NE
Salmon Arm, BC

Zone R8
Max Building Height 6.0m
Max living space 968 SF



⑤ Upper Floor
3/32" = 1'-0"



④ Main Floor
3/32" = 1'-0"

Drawing Index	
Drawing Number	Drawing Name
1	Cover Page and Site Plan
2	Floor Plans
3	Elevations
4	Sections and Details

The building must comply with thermal performance codes in part 9.38 of the British Columbia Building Code (BCBC 9.38)

The building is designed to use a heat recovery system (HRV)

General Notes

All work to be in compliance with the current residential standards of the National Building Code of Canada, current electrical and plumbing codes and all local building code and bylaws which may take precedence

All work shall be performed in all respects to good building practice

Written dimensions to be followed. Do not scale from the drawings

Site

- Building location to be verified by surveyor prior to construction.
- Foundations to be poured on undisturbed engineered building pad.
- All measurements, grades and levels to be verified on site before commencing construction

Framing and Foundations

- All dimensions to face of stud unless otherwise noted.
- All headers, beams, columns and supporting foundations to be confirmed by the roof truss and floor system supplier.
- Roof truss and floor joist layout will be provided by the manufacturer
- All suspended slabs and foundations walls over 6" must be designed and/or approved by a professional engineer.
- All standard foundations to conform with engineering, soil conditions and best practices.
- All concrete to have a minimum compressive strength of 20mpa at 28 days. Do not backfill till foundations until floor structure is complete.
- Any roof truss web configurations shown are for representation only. Engineered drawings of each truss will be supplied by the truss supplier.
- All joists spans more than 7'-0" shall be bridged at mid span using 2x2 diagonal bracing installed as per N.B.C.
- Flashing to be installed at all penetrations in roof system and changes in roof plane.
- Flashing to be installed at all changes in horizontal exterior finishes and over all unprotected openings. Caulking to be installed around all unfinished exterior openings

Thermal Performance

- All new construction must conform to BCBC 9.38 thermal performance codes

Plumbing, Electrical and Mechanical Systems

- All plumbing to be designed and installed per current plumbing codes and by a qualified plumber.
- Confirm layouts and dimensions with suppliers of all kitchens, bars, washrooms, laundry rooms, utility rooms, in-floor heating, etc.
- All fixtures to be approved by the owner.
- Heating and venting calculations and system design by engineer, supplier or qualified installer.
- Electrical system to be designed and installed by qualified electrician

Local Bylaws and Development Regulations

- All construction will be within local bylaw requirements. These include design restrictions, height restrictions, frost coverage, water run off containment, water consumption, landscaping, etc.
- Any changes required to these designs should be reported to Meyer Designs
- Perimeter drainage shall be installed where required by local authorities having jurisdiction and to their approval

Meyer Designs shall not be responsible for any variances from the structural drawings and specifications, or adjustments required resulting from conditions encountered at the job site, and is the sole responsibility of the owner and contractor

Meyer Designs makes every effort to provide complete and accurate construction drawings. However, we assume no liability for errors or omissions which may affect construction. It is the responsibility of all trades and sub trades to check and verify all dimensions and details before commencing with their portion of the construction. Should any discrepancies be found on these plans please advise Meyer Designs so the necessary corrections can be made.

Proposed Residence for Dale and Renee Unruh



② South-East Perspective



③ South-West Perspective

For Permit and Construction v1.1



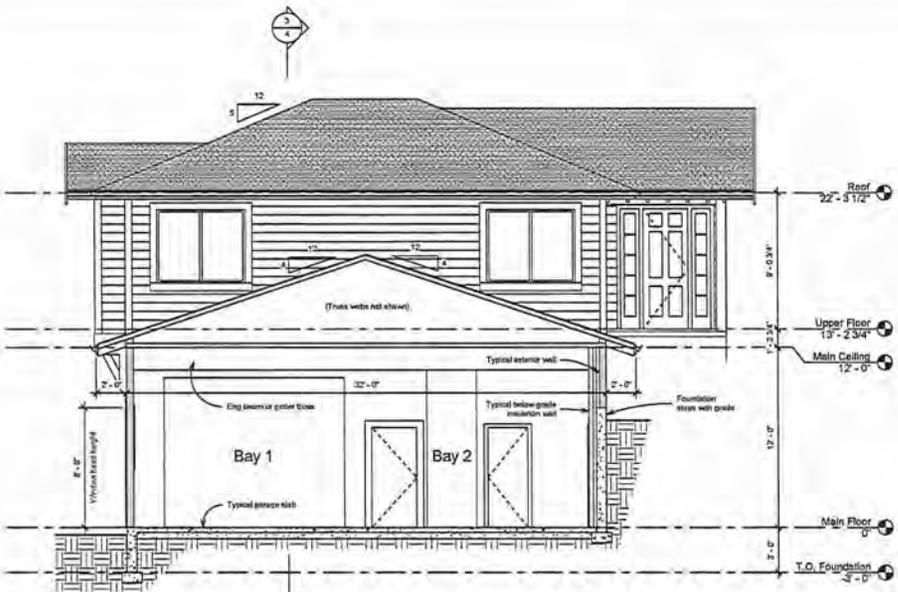
**MEYER
DESIGNS LTD**
800 Mt Tod Drive
Coldstream, BC
V1B 3Y6
250-503-4579
Kelly@MeyerDesigns.ca
MeyerDesigns.ca

Cover Page and Site Plan

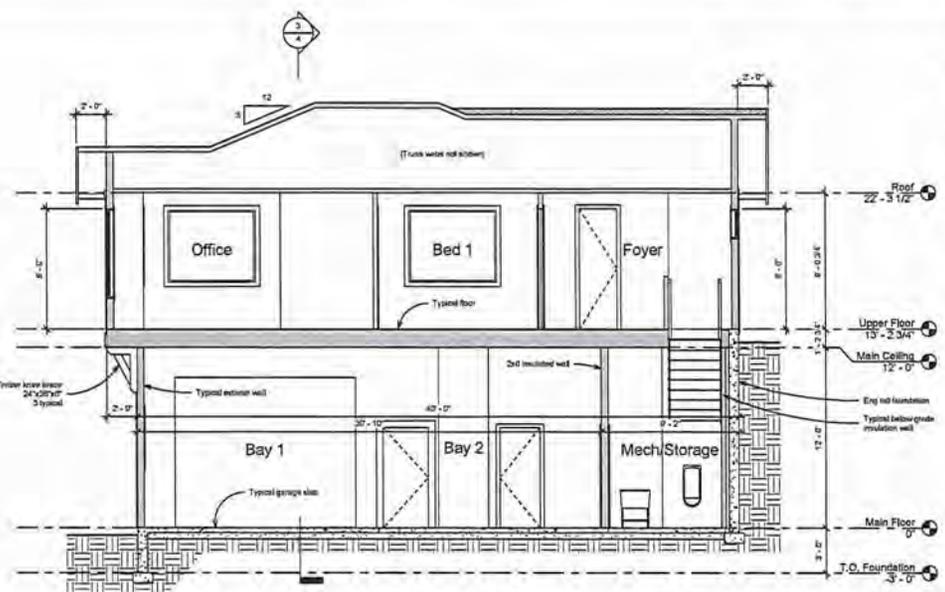
Project number: 2017-043-A
Date: Dec 5, 2017
Drawn by: Kelly Meyer

Unruh Residence
4431, 17 street NE
Salmon Arm, BC

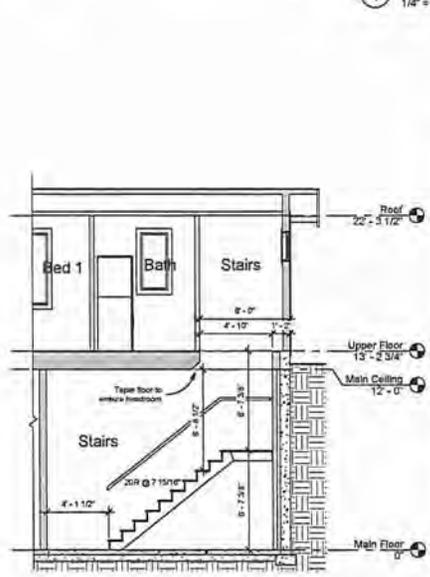
1



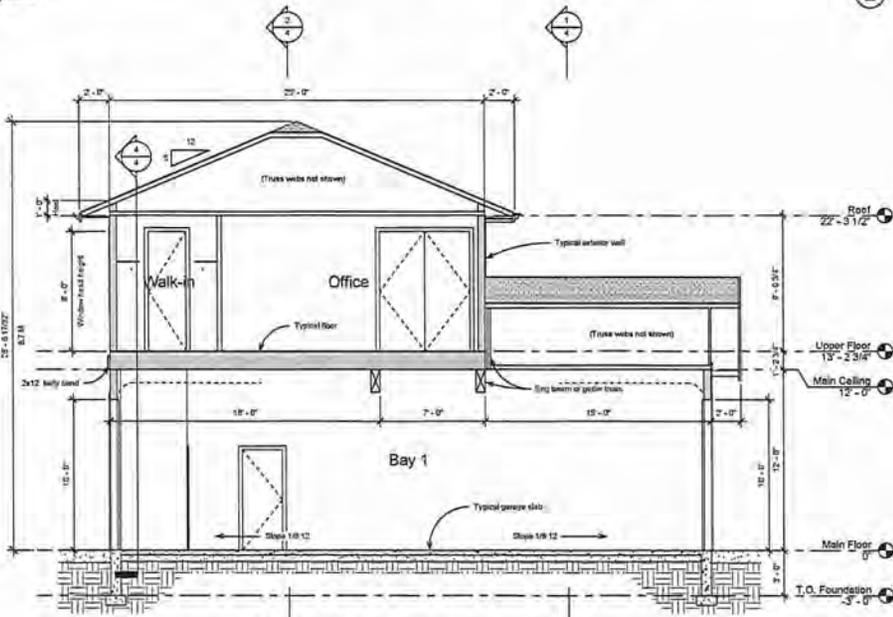
1 Section 1
1/4" = 1'-0"



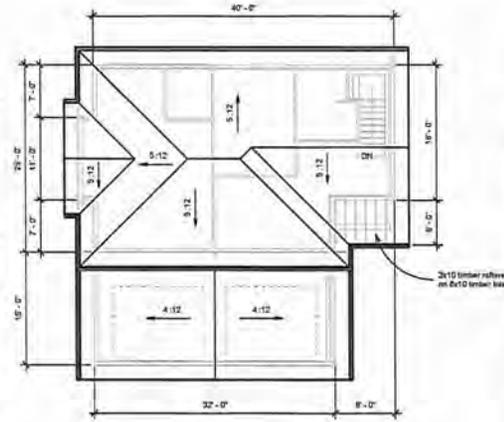
2 Section 2
1/4" = 1'-0"



4 Section 3
1/4" = 1'-0"



3 Section A
1/4" = 1'-0"



5 Roof Plan
1/8" = 1'-0"

Dimensions are to wall or exterior walls unless noted otherwise.
Interior doors, if not dimensioned, are 3" from the corner to R/O or centerline in the wall.

Typical Floors

- Floor over heated space (hallways and stairs)**
 2x4" lag plywood (glued and screwed)
 1/4" eng wood lath by supplier
 Blocking as required by local supplier
 5/8" drywall
- Floor over unheated space (garage bays)**
 2x4" lag plywood (glued and screwed)
 1/4" eng wood lath
 Blocking as required by local supplier
 R4G batt insulation
 2 layers type X drywall

- Garage floor slab**
 4" finished concrete
 Slope garage slabs towards door at 1/8" 12"
 Reinforcing per engineering
 6 mil poly
 5" compacted granular fill
- Deck slab (at front entry)**
 4" finished concrete
 10# bars 24" oc BW
 6 mil poly
 5" compacted granular fill
 Slope 1/8" 12" away from house

Typical Walls

- Foundation wall**
 8" precast concrete per engineering
 Cast-in-place with full-section structural perimeter Rigid (R12) insulation on inside of foot walls
- Exterior wall**
 Cement board or wood shake finish
 On building paper or building wrap
 1/2" plywood
 2x4 stud wall @ 24" oc
 R12 batt insulation
 6 mil poly
 1/2" drywall
- Interior wall**
 2x4 stud wall @ 16" oc
 1/2" drywall both sides
- Water table insulation wall**
 2" precast concrete wall with corresponding 2" air space
 2x4 stud wall @ 16" oc (aligned 0" from center)
 R20 batt insulation (1" air space)
 6 mil poly
 1/2" drywall

Typical Roof

- Flat Roof Trusses**
 2x12 spaced @ 16" oc
 Roof paper
 3/8" sheathing
 Eng wood roof system @ 24" oc
 5/12 with 1/2" heel on upper roof
 4:12 with 1/2" heel on lower garage roof
 R10 insulation at the ceiling in these areas includes garage
 6 mil poly
 5/8" drywall
- Soffit and Fascia**
 2x12 wood lath on gable ends to match finish
 2x4 wood lath
 Vertical aluminum soffit
 5/8" x 3" corr. aluminum gutters

This building must comply with thermal performance tables in part 0.36 of the British Columbia Building Code (BCBC 0.36)

This building is designed to meet a net-zero energy system (NZES)

MEYER DESIGNS LTD
 600 Mt Tod Drive
 Coldstream, BC
 V1B 3Y6
 250-503-4578
 Kelly@MeyerDesigns.ca
 MeyerDesigns.ca

Sections and Details

Project number	2017-043-A
Date	Dec 5, 2017
Drawn by	Kelly Mayer

Urnth Residence
 4431, 17 Street NE
 Salmon Arm, BC

4

For Permit and Construction v1.1

Dimensions are to stud on exterior walls and to center of stud for interior walls.
 Interior doors, if not dimensioned, are 3" from the corner to RO or centered in the wall.

Typical Cladding

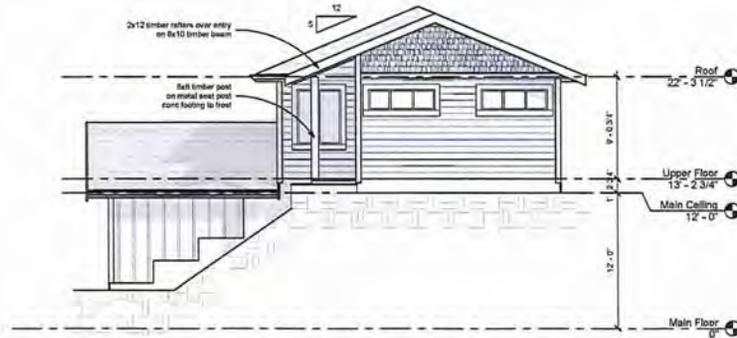
- Horizontal Siding (Upper Floor)**
 Hardie siding - 7' reveal
 1x6 wood trim on corners and windows and doors
- Shakes in gable ends**
 Wood shingles (sloped or straight)
- Vertical Board and Batten (Main Floor)**
 Small panel with 1.5" vertical battens @ 18" oc
- Fascia**
 2x10 wood fascia on gable ends to match house
 2x6 wood fascia c/w 3" rain gutters on flat edges
- Soffits**
 Formed aluminum to match gutters

Windows and Doors

All exterior door and window sizes, patterns, configurations, configurations and separations to be confirmed by window manufacturer to Ensure NAFI (North American Fenestration Standard) compliance.
 What is shown on the plans is a representation of the look and feel intended by the designer.
 The type of window used and manufacturer space may vary and therefore need to be confirmed prior to construction.



① **Front Elevation** *North*
 3/16" = 1'-0"



② **Right Elevation** *East*
 3/16" = 1'-0"



③ **Rear Elevation** *South*
 3/16" = 1'-0"



④ **Left Elevation** *West*
 3/16" = 1'-0"

Height of Accessory Building = 8.7 m



**MEYER
DESIGNS LTD**
 600 Mt Tod Drive
 Coldstream, BC
 V1B 3Y6
 250-503-4579
 Kelly@MeyerDesigns.ca
 MeyerDesigns.ca

Elevations

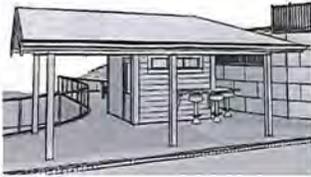
Project number	2017-043-A
Date	Dec 5, 2017
Drawn by	Kelly Meyer

Unruh Residence 4431, 17 street NE Salmon Arm, BC	3
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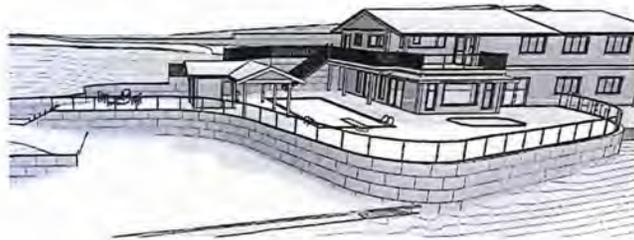
For Permit and Construction v1.1



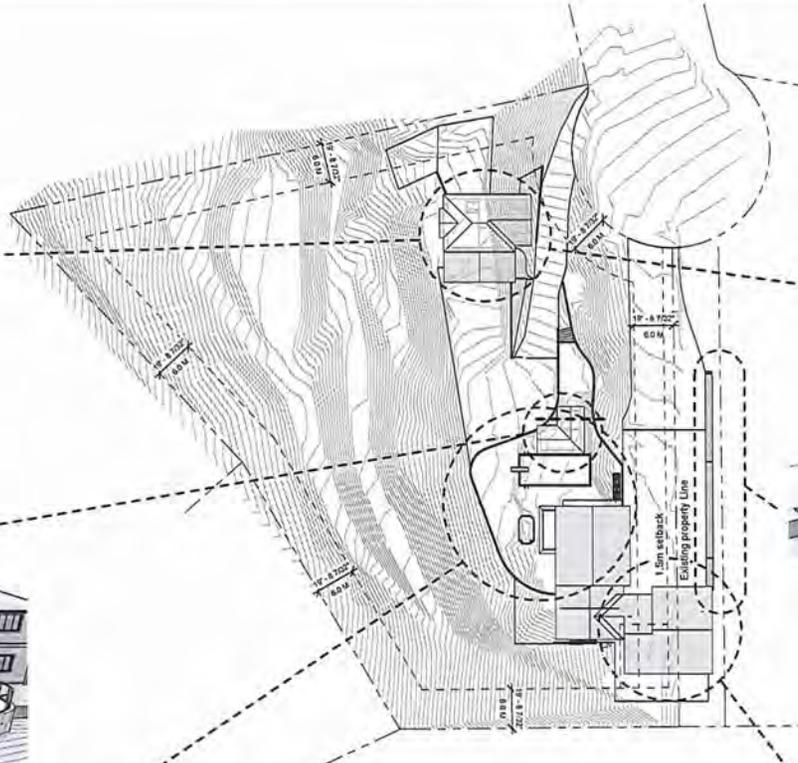
③ Carriage House - See Project 2017-043-A



⑤ Poolside Cabana - See Project 2017-043-B



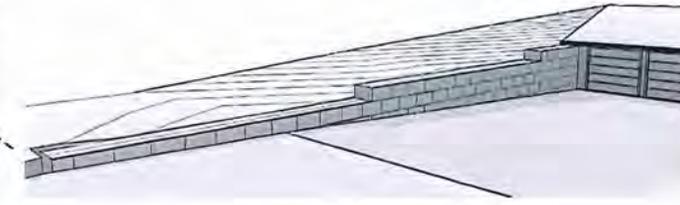
② Pool Deck Retaining - See sheet 2



① Site Plan - Overall
1" = 30'-0"



⑧ Carriage House (North) - See 2017-043-A



④ Garage Addition Retaining - See sheet 3



⑦ 3 Bay Garage Addition - See sheet 3

Drawing Index	
Drawing Number	Drawing Name
1	Cover Page and Site Plan
2	Pool Area Retaining Plans
3	Carriage House and Garage Addition Retaining
4	3 Bay Garage Addition

General Notes

All work to be in compliance with the current residential standards of the National Building Code of Canada, current electrical and plumbing codes and all local building code and bylaws which may take precedence.

All work shall be performed in all respects to good building practice

Written dimensions to be followed. Do not scale from the drawings

- Site**
- Building location to be verified by surveyor prior to construction.
 - Foundations to be poured on undisturbed engineered building pad.
 - All measurements, grades and levels to be verified on site before commencing construction

- Framing and Foundations**
- All dimensions to face of stud unless otherwise noted.
 - All headers, beams, columns and supporting foundations to be confirmed by the roof truss and floor system supplier.
 - Roof truss and floor joist layout will be provided by the manufacturer
 - All suspended slabs and foundations walls over 8' must be designed and/or approved by a professional engineer.
 - All standard foundations to conform with engineering, soil conditions and best practices.
 - All concrete to have a minimum compressive strength of 20mpa at 28 days. Do not backfill till foundations are complete.
 - Any roof truss web configurations shown are for representation only. Engineered drawings of each truss will be supplied by the truss supplier.
 - All joists spans more than 7'-0 shall be bridged at mid span using 2x2 diagonal bracing installed as per N.B.C.
 - Flashing to be installed at all penetrations in roof system and changes in roof plane.
 - Flashing to be installed at all changes in horizontal exterior finishes and over all unprotected openings. Caulking to be installed around all unflashed exterior openings

Thermal Performance

- All new construction must conform to BCBC 9.36 thermal performance code

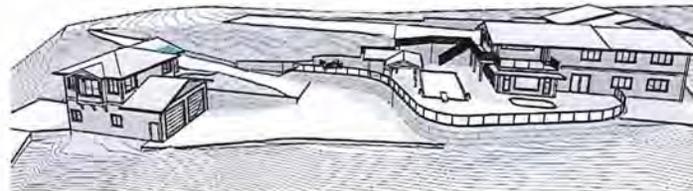
- Plumbing, Electrical and Mechanical Systems**
- All plumbing to be designed and installed per current plumbing codes and by a qualified plumber.
 - Confirm layouts and dimensions with suppliers of all kitchens, bars, washrooms, laundry rooms, utility rooms, in-floor heating, etc.
 - All fixtures to be approved by the owner.
 - Heating and venting calculations and system design by engineer, supplier or qualified installer.
 - Electrical system to be designed and installed by qualified electrician

- Local Bylaws and Development Regulations**
- All construction will be within local bylaw requirements. These include design restrictions, height restrictions, front coverage, water run off containment, water consumption, landscaping, etc.
 - Any changes required to these designs should be reported to Meyer Designs
 - Perimeter drainage shall be installed where required by local authorities having jurisdiction and to their approval

Meyer Designs shall not be responsible for any variances from the structural drawings and specifications, or adjustments required resulting from conditions encountered at the job site, and is the sole responsibility of the owner and contractor

Meyer Designs makes every effort to provide complete and accurate construction drawings. However, we assume no liability for errors or omissions which may affect construction. It is the responsibility of all trades and sub trades to check and verify all dimensions and details before commencing with their portion of the construction. Should any discrepancies be found on these plans please advise Meyer Designs so the necessary corrections can be made.

Proposed Development for Dale and Renee Unruh



⑥ Site Perspective

Legal Address
Lot 3
Plan 40035

Civic Address
4431 17th Street NE
Salmon Arm, BC

Currently zoned R7

Note
Application to move East property line and change to zone R8 is in progress



MEYER
DESIGNS LTD
600 Mt Tod Drive
Coldstream, BC
V1B 3Y6
250-503-4579
Kelly@MeyerDesigns.ca
MeyerDesigns.ca

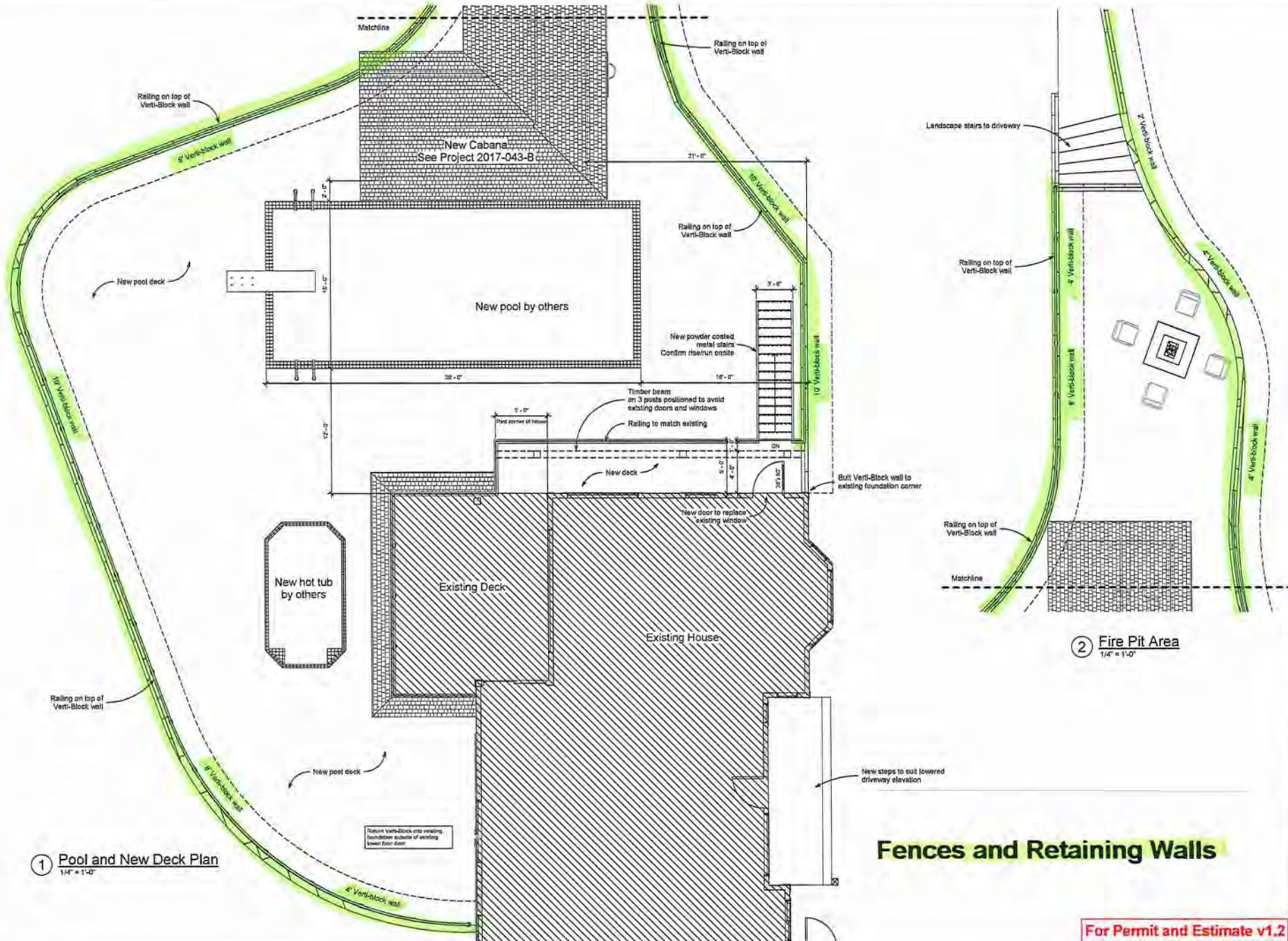
Cover Page and Site Plan

Project Number	2017-043
Date	Jan 30, 2018
Drawn by	Kelly Meyer

Unruh Residence
4431 17th St NE
Salmon Arm BC

1

For Permit and Estimate v1.2



① Pool and New Deck Plan
1/4" = 1'-0"

② Fire Pit Area
1/4" = 1'-0"

Fences and Retaining Walls

For Permit and Estimate v1.2



**MEYER
DESIGNS LTD**

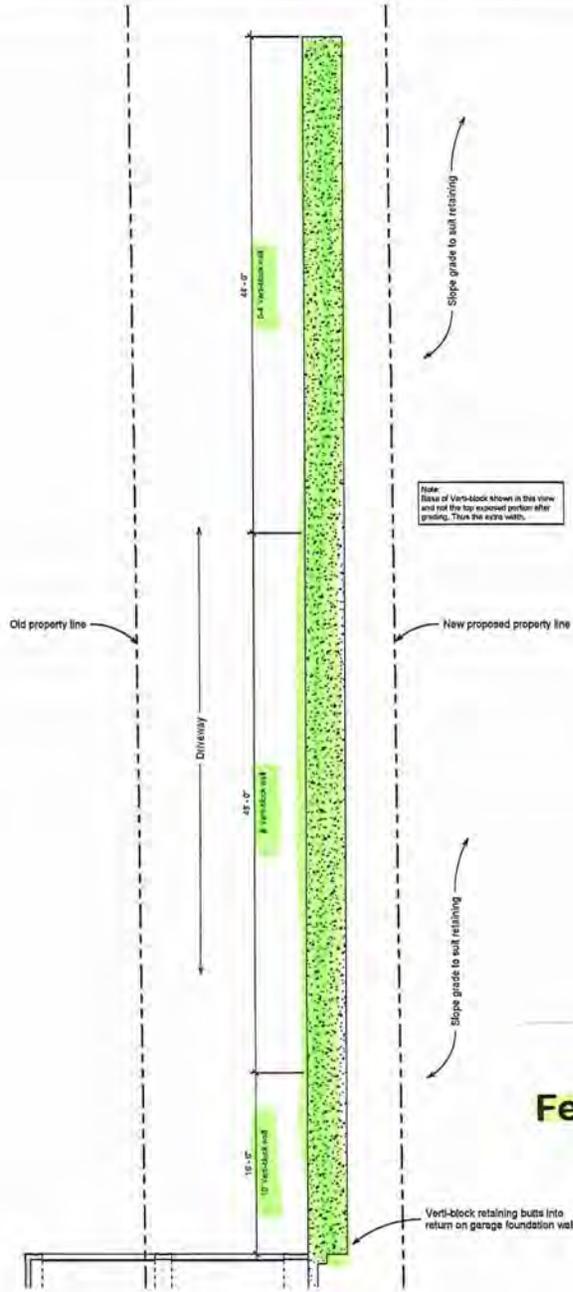
800 Mt Tod Drive
Coldstream, BC
V1B 3Y6

250-503-4578
Kelly@MeyerDesigns.ca
MeyerDesigns.ca

Project number: 2017-043
Date: Jan 30, 2018
Drawn by: Kelly Meyer

Linnuh Residence
4431 17th St NE
Salmon Arm BC.

2



② Garage Addition Retain
3/16" = 1'-0"



Fences and Retaining Walls

① Carriage House Retaining Plan
3/16" = 1'-0"

For Permit and Estimate v1.2



MEYER
DESIGNS LTD

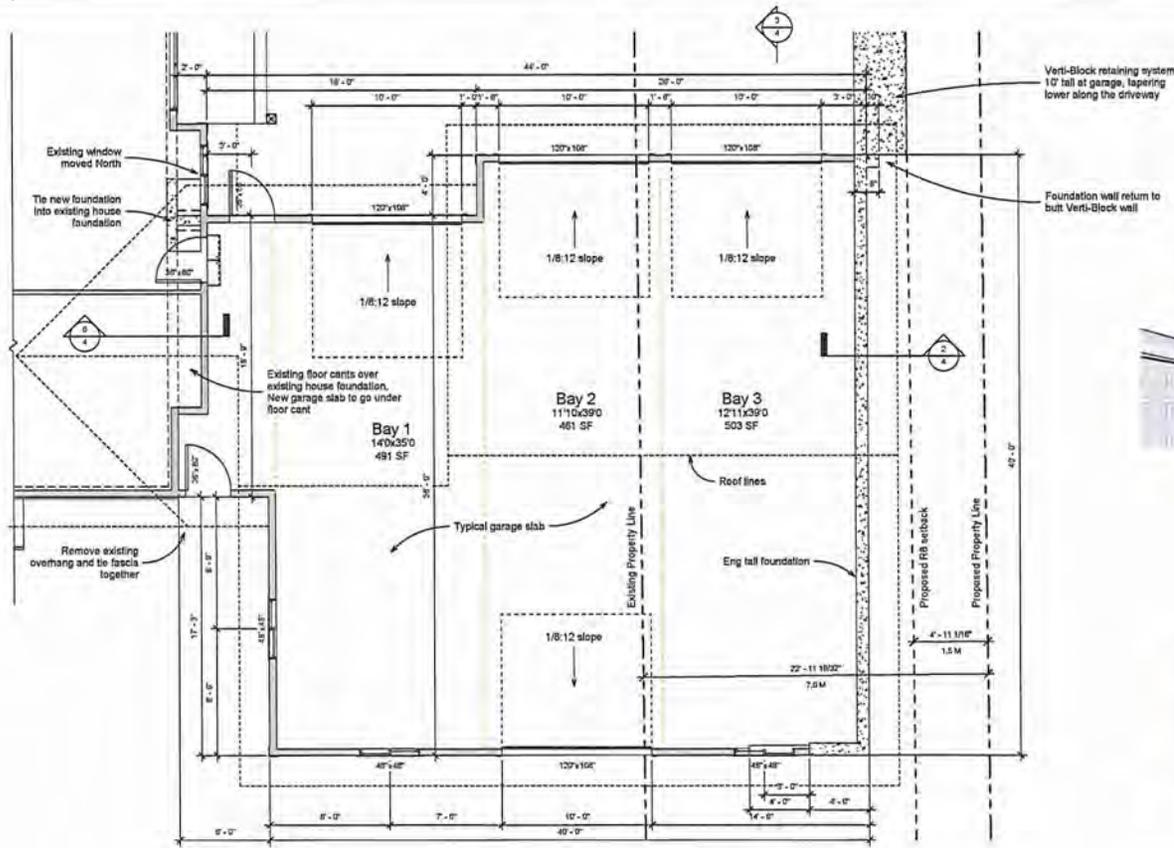
600 Mt Tod Drive
Coldstream, BC
V1B 3Y6

250-503-4578
Kelly@MeyerDesigns.ca
MeyerDesigns.ca

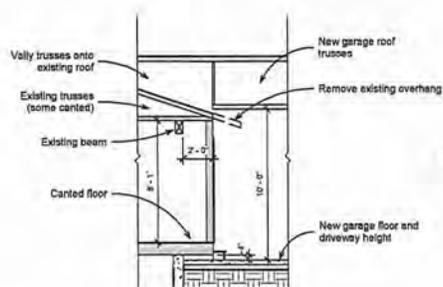
Carriage House and Garage Addition Retaining

Project Number:	2017-043
Date:	Jan 30, 2018
Drawn by:	Kelly Meyer

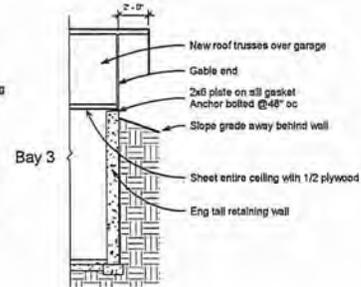
Unruh Residence 4431 17th St NE Salmon Arm BC	3
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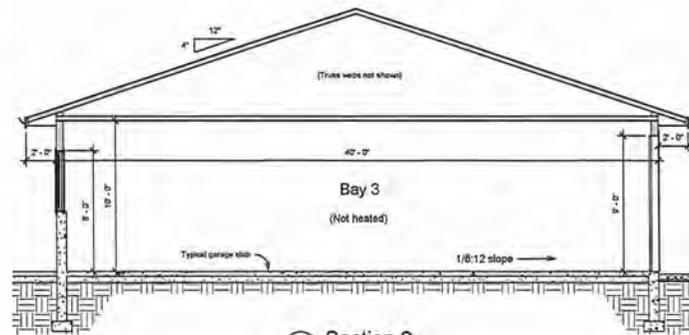
1 Garage Floor Plan
1/4" = 1'-0"



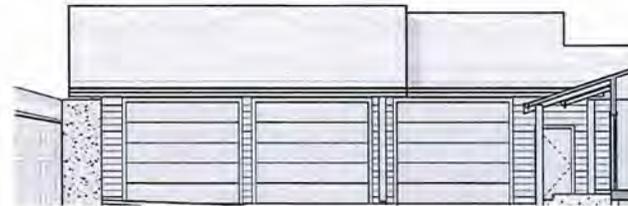
6 Section 3
1/4" = 1'-0"



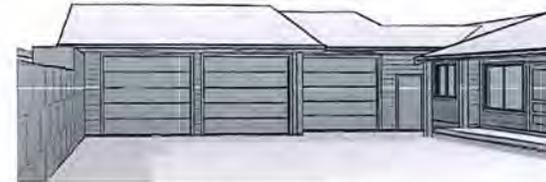
2 Section 1
1/4" = 1'-0"



3 Section 2
1/4" = 1'-0"



5 Garage Front Elevation
3/16" = 1'-0"



4 Front Perspective

Dimensions are to stud or exterior walls and to center of door or side of stud for interior walls.
 (Slender doors, if not dimensioned, are 2" from the corner to R/O as centered in the wall.)

Typical Floors

Garage floor slab
 4" finished concrete
 Slope garage slabs towards door at 1/8:12
 Reinforcing per engineering
 0.1 mil poly
 5" compacted granular fill

Typical Walls

Exterior wall
 Current sheet or select similar finish
 On building paper or building wrap
 1/2" plywood
 2x6 stud wall @ 16" oc

Typical Roof

Structural Roof Trusses
 Asphalt shingles (Min 25 year) to match existing
 Roof paper
 2x12 sheathing
 Eng wood roof trusses @ 24" oc
 4:12 with 1/2" lead

Trim and Fascia
 Match to existing house
 1x4" coat, aluminum gutters

Typical Cladding

Horizontal Siding
 Hardie siding - 7" reveal
 1x6 wood trim on corners and windows and doors
 Match to existing house



600 Mt Tod Drive
 Coldstream, BC
 V1B 3Y6
 250-503-4575
 Kelly@MeyerDesigns.ca
 MeyerDesigns.ca

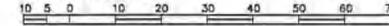
3 Bay Garage Addition

Project number 2017-043
 Date Jan 30, 2018
 Drawn by Nathan Meyer

Linnh Residence
 4421 17th St NE
 Salmon Arm BC

Sketch Plan of Proposed Subdivision of Lots 2, 3 and 4, Sec 25, Tp 20, R 10, W6M, KDYD, Plan KAP40035

BCGS 82L.074



All distances are in metres.
The intended plot size of this plan is 560mm in width by 432mm in height (C size) when plotted at a scale of 1:250



LEGEND

- edge of driveway
- driveway
- chainlink fence

December 19, 2017

BROWNE JOHNSON LAND SURVEYORS
B.C. AND CANADA LANDS
SALMON ARM, B.C. 250-832-9701
File: 656-17 656-17.raw

Wes Miles

Subject: RE: Variance Application VP-468

From: Don Hindbo [<mailto:don@hindboconstruction.com>]

Sent: November-28-17 1:14 PM

To: Denise Ackerman

Subject: RE: Variance Application VP-468

Denise

The reason for this Height variance , is that the owners Dale & Renee Unruh , have applied to build a cairrage home on the property , & they way the ground slopes , the building has a height of 28 ft 6.5 inches & the maximum allowed under R-8 is 24.6 ft , so we need a variance for roughly 4 feet

Thanks

Don Hindbo

Hindbo Construction Group Inc.

2691 13 ave sw

Salmon Arm B C

Office-250 832 1017

Cell- 250-804-3038

don@hindboconstruction.com



APPENDIX 7



Photo 1: Photo looking west from 17 Street NE at the accessory building / future detached suite.



Photo 2: Photo looking south at the existing dwelling and driveway access.



City of Salmon Arm
Development Services Department Memorandum

TO: Her Worship Mayor Cooper and Council

FROM: Development Services Department

DATE: February 21, 2018

SUBJECT: Development Variance Permit Application No. VP-469
Lot B, Plan 26672 Sec. 19, Tp. 20, R. 9, W6M, KDYD
2820 - 30 Street NE
Owner: B. McKinnon
Agent: R. Walters

Motion for Consideration

THAT: Development Variance Permit No. VP-469 be issued for Lot B, Plan 26672, Sec. 19, Tp. 20, R. 9, W6M, KDYD to vary the provisions of Zoning Bylaw No. 2303 as follows:

1. **Section 4.9.1.1** - decrease the minimum setback requirement from the centre line of an Arterial Street (30 Street NE) for a principal building on proposed Lot 1, as shown on Appendix 3, from 18.5 metres to 17.0 metres.

Staff Recommendation

THAT: The Motion for Consideration be adopted.

Proposal

The subject property is located at the intersection of 30 Street NE and 28 Avenue NE. The owner has received preliminary approval to subdivide the property into three parcels and wishes to construct a residence on one of the new parcels (proposed Lot 1) that will extend into the minimum setback requirement along 30 Street NE. A location map, ortho photo, site plan and an outline of the proposal from the owner's agent are attached as Appendices 1 through 4.

Background

The property is designated Low Density Residential in the Official Community Plan and is zoned R-1 (Single Family Residential).

The property has frontage on both 28 Avenue NE, which is designated as a Local Street, and 30 Street NE, which is designated as an Arterial Street. Local Streets have an ultimate right of way of 20.0 metres (10.0 metres from centreline) and Arterial Streets have an ultimate right of way width of 25.0 metres (12.5 metres from centreline).

In the R-1 Zone, the minimum setback requirement along an Exterior Side Parcel Line is 6.0 metres. However, if the adjacent roadway is a designated Arterial Street, an additional setback of 18.5 metres from the centreline of the roadway is also applied. This additional setback allows for widening of the Arterial Street to its ultimate 12.5 metre width from centreline and ensures that new buildings will still be setback 6.0 metres from the External Side Parcel Line following the road dedication.

As shown on the site plan in Appendix 3, proposed Lot 1 has an External Side Parcel line adjacent to 30 Street NE. This lot will be approximately 453 square metres in size and is currently vacant. With the required setback of 18.5 metre setback from centreline, the building envelope on this parcel will be limited to a width of 7.7 metres. The owner would like to increase this width by 1.5 metres to allow more flexibility in preparing a building floor plan. The additional width to the building envelope will reduce the setback from centreline from 18.5 metres to 17.0 metres.

Site Context

Adjacent land uses include the following:

North: Residential lot zoned R-1(Single Family Residential)
 South: 28 Avenue NE, then residential lot zoned R-1 (Single Family Residential)
 East: Residential lot zoned R-8 (Residential Suite)
 West: 30 Street NE; then residential lot zoned R-1(Single Family Residential)

Staff have reviewed the proposal and provide the following:

Fire Department

No concerns.

Building Department

No concerns.

Engineering Department

See Appendix 5.

Planning Department

Setback requirements prevent owners from crowding neighbouring properties, ensure fire separation distances are maintained and traffic site lines are not impeded. With this application, all other setback requirements will be adhered to and the proposed building will still have a 7.0 metre setback from the existing Exterior Side Parcel Line.

The requested variance is relatively minor and will not impact traffic site lines. The variance will have little, if any impact on neighbouring properties and the proposed residence will not exceed the maximum permitted lot coverage provisions in the R-1 Zone (45%).

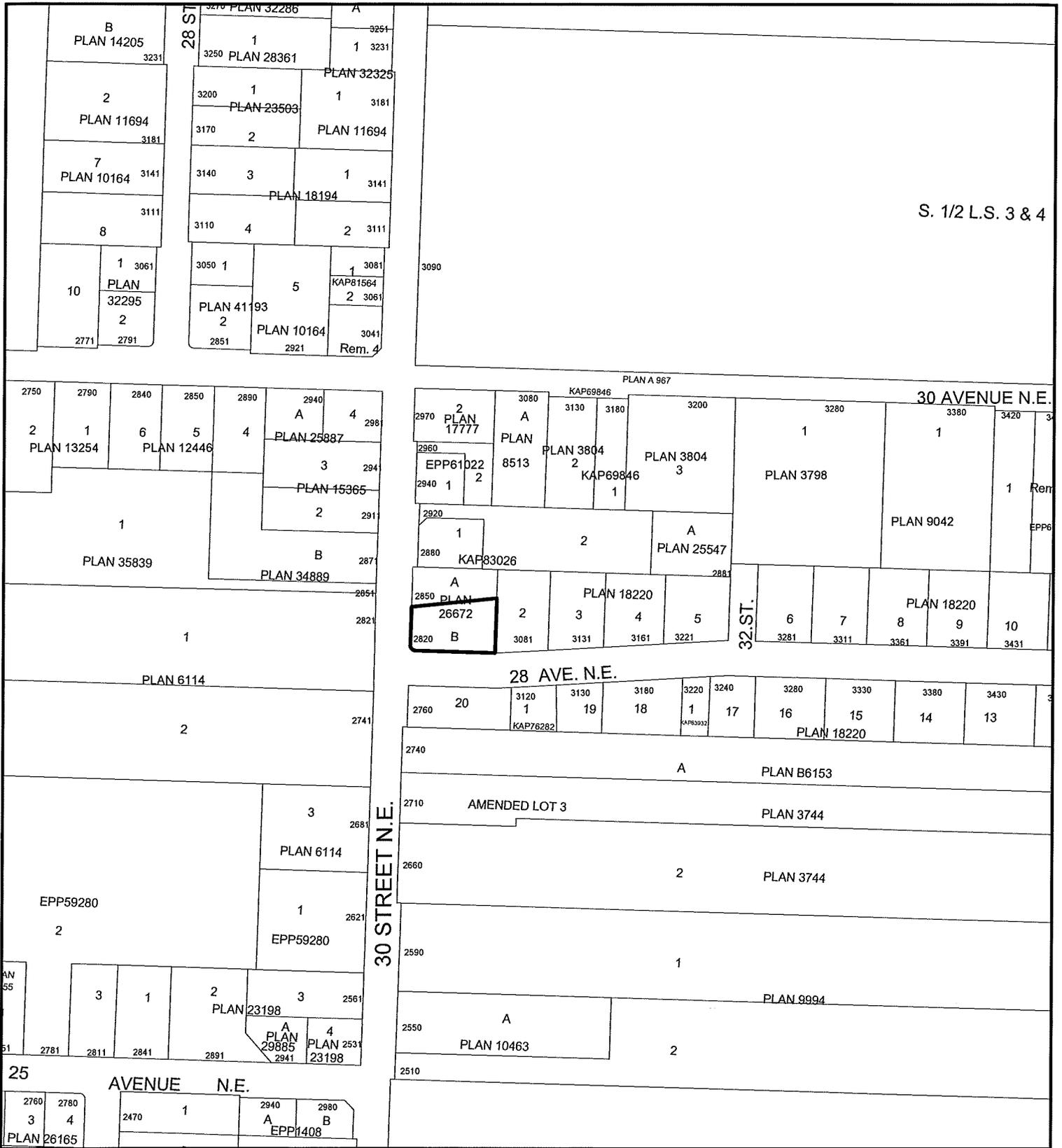
As 30 Street NE is currently dedicated 10 metres from centreline at this location, the applicants are not required to provide any additional dedication in conjunction with their proposed subdivision. However, the applicants have agreed to provide a 2.5 metre wide Road Reserve along the External Side Parcel Line which would allow the City to widen the roadway, at no additional cost, should it be needed in the future.


 Prepared by: Jon Turlock
 Planning & Development Officer


 Reviewed by: Kevin Pearson, MCIP
 Director of Development Services

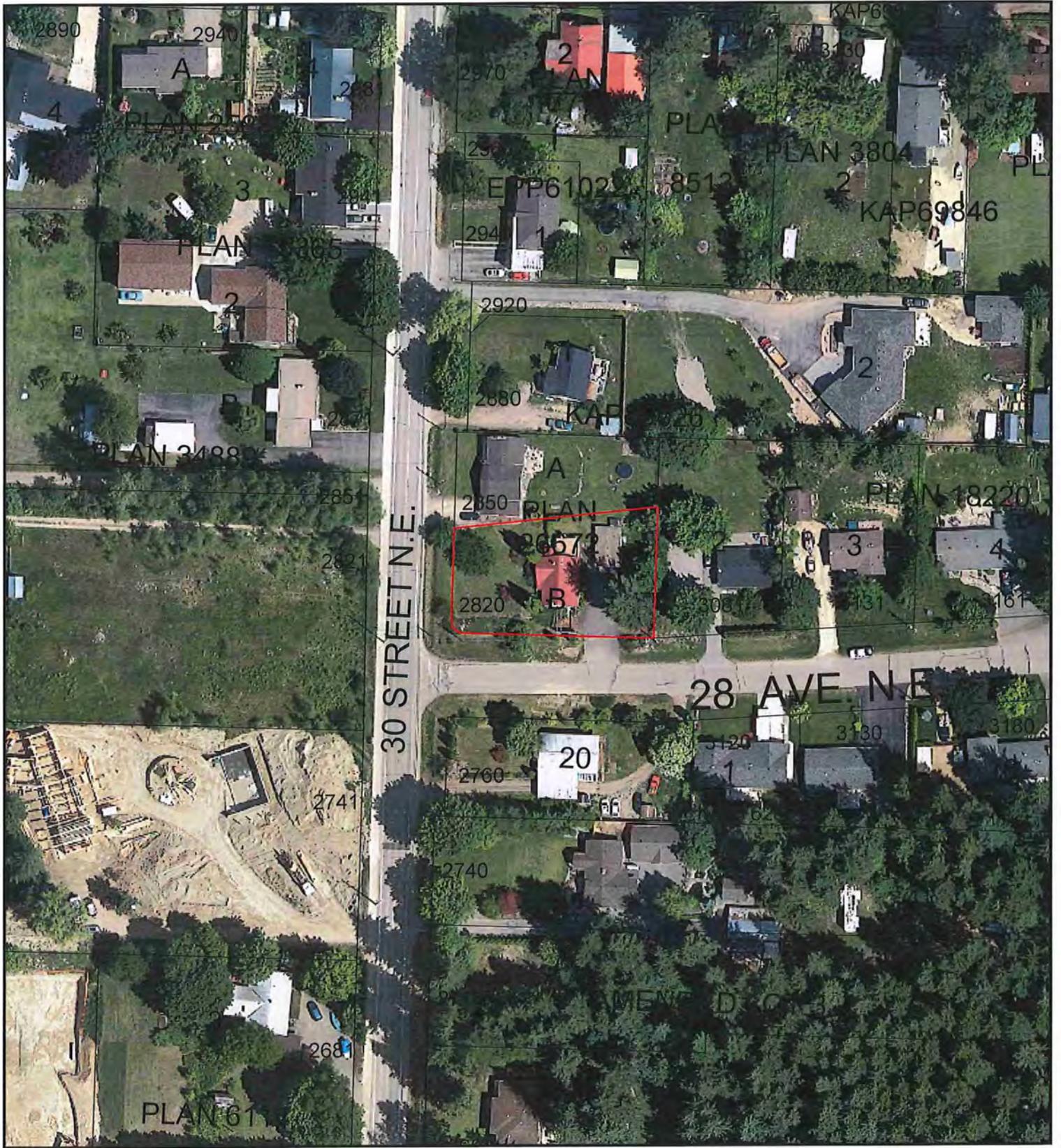
Appendices

1. Location map
2. Ortho photo
3. Site plan
4. Owner's letter
5. Engineering Dept. comments



 Subject Property

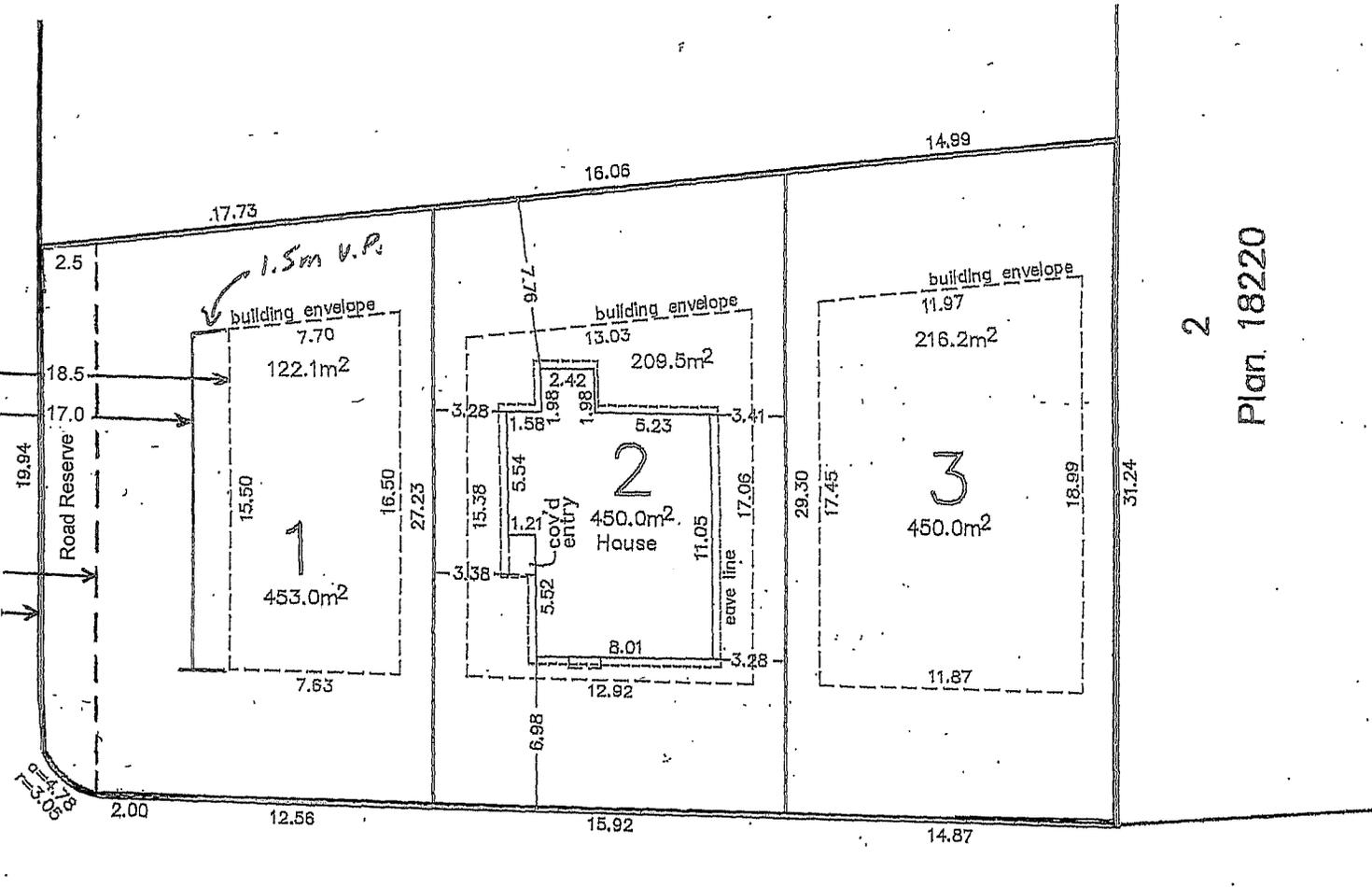
Location Map



 Subject Property

30 Street NE

centreline



28 Avenue NE

2
Plan. 18220

December 6, 2016

BROWNE JOHNSON LAND SURVEYORS
 B.C. AND CANADA LANDS
 SALMON ARM, B.C. Ph.250-832-9701
 File: 620-16 . . . 620-16.raw

APPENDIX 3

City of Salmon Arm Planning dept.

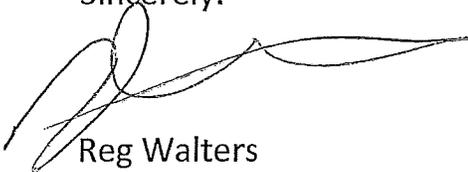
Feb. 08/ 2018

Explanatory letter regarding the request for a Variance to the City of Salmon Arm Zoning Bylaw.

This letter is to further clarify our request for a Variance Permit on the lot at NE corner of the intersection of 30 St and 28 Ave. NE. (address pending subdivision approval) We are wanting a Variance from 18.5m to 17.0m for the setback from centerline of an Arterial Collector which 30th St NE is classified as. This will allow us to construct a 30 ft. wide Single Family Dwelling as opposed to a 25 foot wide (7.63m) home which is difficult to create desirable floor plan and to sell.

We thank you for your time and consideration of our request.

Sincerely.

A handwritten signature in black ink, appearing to read 'Reg Walters', with a long horizontal flourish extending to the right.

Reg Walters



City of Salmon Arm
Memorandum from the Engineering
and Public Works Department

To: Kevin Pearson, Director of Development Services
 Date: February 9, 2018
 Prepared by: Darin Gerow, Engineering Assistant
 SUBJECT: **DEVELOPMENT VARIANCE NO. VP- 469E**
 LEGAL: Lot B, Section 19, Township 20, Range 9, W6M, KDYD, Plan 26672
 CIVIC: 2820 – 30 Street NE
 Owner: McKinnon, B./Walters, R., - 1381 – 16 Street NE, Salmon Arm, V1E 2T8
 Agent: Owner

Further to your referral dated January 19, 2018, the Engineering Department has thoroughly reviewed the site and offers the following comments and recommendations, relative to the variances requested:

The applicant is requesting to vary City of Salmon Arm Zoning Bylaw No. 2303 Section 4.9.1.1 as follows:

Reduce the building setback from centre line on an Arterial street from 18.5 meters to 17.0 meters.

Engineering Department does not have any concerns with the proposed setback reduction, and therefore recommends this variance be granted.

Darin Gerow, ASCT
 Engineering Assistant

Jenn Wilson, P. Eng.
 City Engineer